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PLANNING

DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH

Junction Options Assessment

Prepared for:
Meath County Council



comhairle chontae na mí
meath county council

Date: May 2024

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Junction Options Assessment

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Abstract: This report contains a junction options assessment to assist the preparation of a planning application for development at Lough Bracken, Co. Meath.

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1.1 Introduction

The purpose of this report is to provide an assessment of the junction between Nobber Road and the proposed development access to Lough Bracken in Co. Meath, following TII technical standards for Geometric Design of Junctions (DN-GEO-03060) and Rural Road Link Design (DN-GEO-03031).

The analysis includes an assessment of the proposed access to determine the extents of the existing junction sight distance for vehicles emerging from the Lough Bracken development and an assessment of stopping sight distance for vehicles travelling along Nobber Road. This report outlines potential mitigation measures that could be implemented at the development access to improve road user safety. The site location is shown in Figure 1-1 below.



Figure 1-1: Site Location

1.2 Existing Site Conditions

1.2.1 Road Width

Nobber Road is a two-lane single carriageway with varying width of between approximately 5m and 6m. At the Nobber Road/Lough Bracken junction a localised widening of Nobber Road increases the available width to between 7 and 10m at the mouth of the junction.

The width of the access road to Lough Bracken is between 4m and 5m at the junction with Nobber Road.



1.2.2 Horizontal Alignment

The Nobber Road/Lough Bracken junction is located on the outside of a horizontal curve with an approx. radius of 50m. The western approach to the junction is relatively straight for over 200m. The eastern approach contains a short straight approx. 40m in length immediately east of the junction followed by a radius of approx. 100m and another straight of approx. 150m. The 50m and 100m radius bends, separated by the short straight, form a broken back curve. Figures 1-2 and 1-3 show the immediate approach to the development junction along Nobber Road when looking from the west and the east respectively.

The access road to Lough Bracken contains a short straight of approx. 15m leading from the junction followed by an approx. 35m radius curve and a long straight of 250m. Figure 1-4 shows the development access road junction as viewed from Nobber Road.

1.2.3 Vertical Alignment

From the development junction, Nobber Road grades down at approx. 2.0% - 2.5% to the west and down at approx. 2.5% - 3.0% to the east.

The access road to Lough Bracken grades down at approx. 6% from the junction with Nobber Road.

1.2.4 Junctions/Accesses

There are 7 accesses onto Nobber Road within the vicinity of the proposed development junction, one to the west and 6 to the east. On the western approach there is a field access on the south side of Nobber Road. On the eastern approach there are two field accesses and one house/farm access on the south side of Nobber Road and two field accesses and one house access on the north side.

1.2.5 Linemarking and Signage

No linemarking is present within the site extents. There are two warning signs (W 053R: Series of Sharp Bends), on the approach to the proposed development junction, one on the eastbound approach and one on the westbound approach. Typically, these signs indicate whether the change of direction is to the left or right and therefore, as both signs are the same, an incorrect sign is used on the eastbound approach. Opposite the existing Lough Bracken junction is a single signpost which supports three sign faces - a tourist attraction sign for Lough Bracken, a directional sign for Drumconrath to the east and another directional sign for Nobber to the west.

1.2.6 Roadside Environment

There are no hard strips or hard shoulders present along Nobber Road. The roadside environment includes vegetation that can become overgrown during the summer months, and which reduces visibility distances and the perceived road width. There are no pedestrian or cyclist facilities. There are existing overhead lines in place along Nobber Road which traverse the Nobber Road/Lough Bracken junction.



Figure 1-2: Western Approach to Nobber Road/Lough Bracken Junction



Figure 1-3: Eastern Approach to Nobber Road/Lough Bracken Junction



Figure 1-4: Access to Lough Bracken from Nobber Road

1.2.7 Speed Limit

Nobber Road is a local road in a rural area with a posted speed limit of 80km/h. Table 1-1 shows the design speed corresponding to a posted speed limit as defined in TII Rural Road Link Design standard DN-GEO-03031. For an 80km/h speed limit, the related design speed is 85km/h.

Table 1-1: Maximum Design Speeds for Mandatory Speed Limits

Speed Limit (km/h)	Design Speed (km/h)
50	60
60	70
80	85
100	100
120	120

1.2.7.1 *Speed Limit Assessment*

The road alignment shall be designed so as to ensure that standards of curvature, visibility, superelevation, etc. provide for a Design Speed which shall be consistent with the anticipated vehicle speeds on the road. Design speed is related to road characteristics and is not directly related to mandatory speed limits. A relatively straight alignment in flat country will generate higher speeds, and thus produce a higher Design Speed than a more sinuous alignment in hilly terrain, or amongst dense land use constraints.



Vehicle speeds vary according to the impression of constraint that the road alignment and layout impart to the driver. This constraint can be measured by the following three factors:

- Alignment Constraint (Ac) - Measures the degree of constraint imparted by the road alignment.
- Layout Constraint (Lc) - Measures the degree of constraint imparted by the road cross section, verge width and frequency of junctions and accesses.
- Mandatory Speed Limits - Mandatory Speed Limits are set below the Design Speed and are indicated in Table 1-1.

As set out in TII Rural Road Link Design DN-GEO-03031, where the improved length of existing Regional or Local Road is less than 2km, the Design Speed can be derived using the Alignment Constraints (Ac) and Layout Constraints (Lc) measured over a minimum length of 2km.

A speed assessment of the existing road alignment for Nobber Road was undertaken over a 2km length centred at the Nobber Road/Lough Bracken junction in accordance with DN-GEO-03031. The design speed was determined to be 70km/h which relates to a 60km/h speed limit. Details of the calculations are included in the Design Speed Assessment in Appendix A.

1.2.7.2 Speed Surveys

Traffic data was obtained from an Automatic Traffic Counter (ATC) located on Nobber Road approx. 50m to the west of the Nobber Road/Lough Bracken junction as shown by the blue marker in Figure 1-5. The data was gathered over a 1-week period between Thursday 29th February and Wednesday 6th March 2024. A total of 4,061 vehicles were recorded within the count period. The data obtained was for vehicles travelling in both directions, i.e. eastbound (from B towards A) and westbound (from A towards B). The traffic survey data is included in Appendix B.

The data provided indicates that the 85% speed in the eastbound direction (B to A) was 65.2km/h and in the westbound direction (A to B) was 58.5km/h.

Average traffic speeds were 56.6km/h for eastbound vehicles and 51.8km/h for westbound vehicles. The maximum speeds recorded were 87.8km/h for eastbound vehicles and 75.1km/h for westbound vehicles.

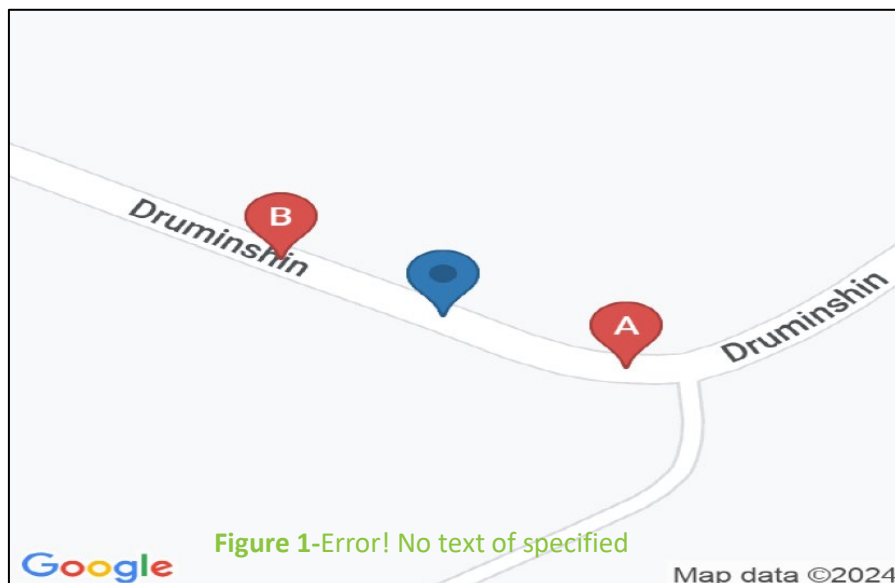


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Table 1-2 provides an overview of the vehicle counts per day travelling eastbound and westbound at the count location along with the 85th percentile speed recorded on each survey day. Eastbound vehicles were reported to have slightly higher speeds than westbound vehicles. This is likely due to the geometry of the road as westbound vehicles will typically slow down as they navigate the bend at the Lough Bracken junction, while eastbound traffic is approaching the bend from a straight section of road.

Table 1-2: Daily Vehicle Numbers & 85th Percentile Speeds (29th Feb to 6th March 2024)

Day	Eastbound Vehicle Numbers	85th Percentile Speeds (km/h)	Westbound Vehicle Numbers	85th Percentile Speeds (km/h)
Thursday	319	65.5	330	59.0
Friday	327	65.7	316	58.4
Saturday	273	66.4	272	57.1
Sunday	189	64.8	190	57.3
Monday	295	63.7	282	58.6
Tuesday	331	65.2	329	59.5
Wednesday	311	64.9	297	58.7

A graph of vehicles numbers versus speed is shown below in Figure 1-6 for eastbound movement. This graph shows that approximately 99.5% of vehicle speeds travelling in the eastbound direction were below the 80km/h posted speed limit with 0.5% travelled between 80 and 90km/h.

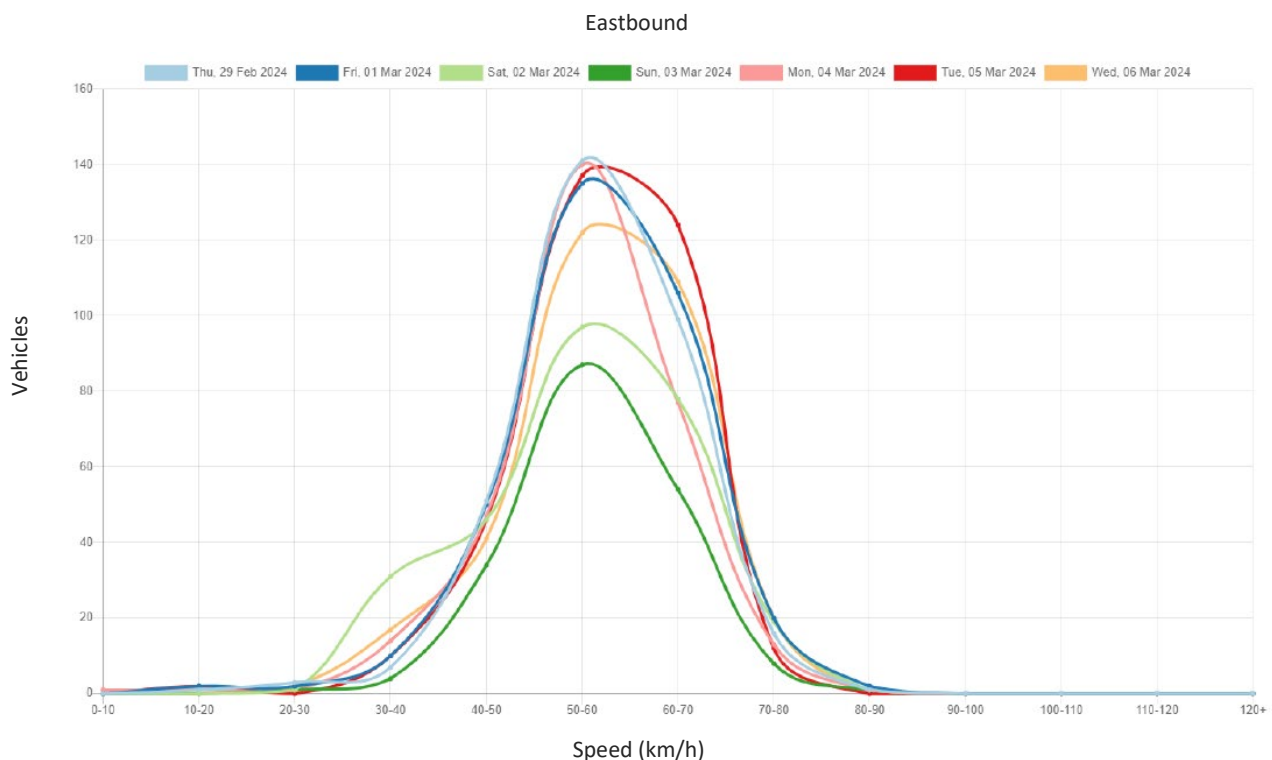


Figure 1-6: Traffic Speed Distribution for Eastbound Vehicles



Figure 1-7 shows the graph of vehicles numbers versus speed for westbound movement. For the duration of the survey period, all vehicles remained within the speed limit.

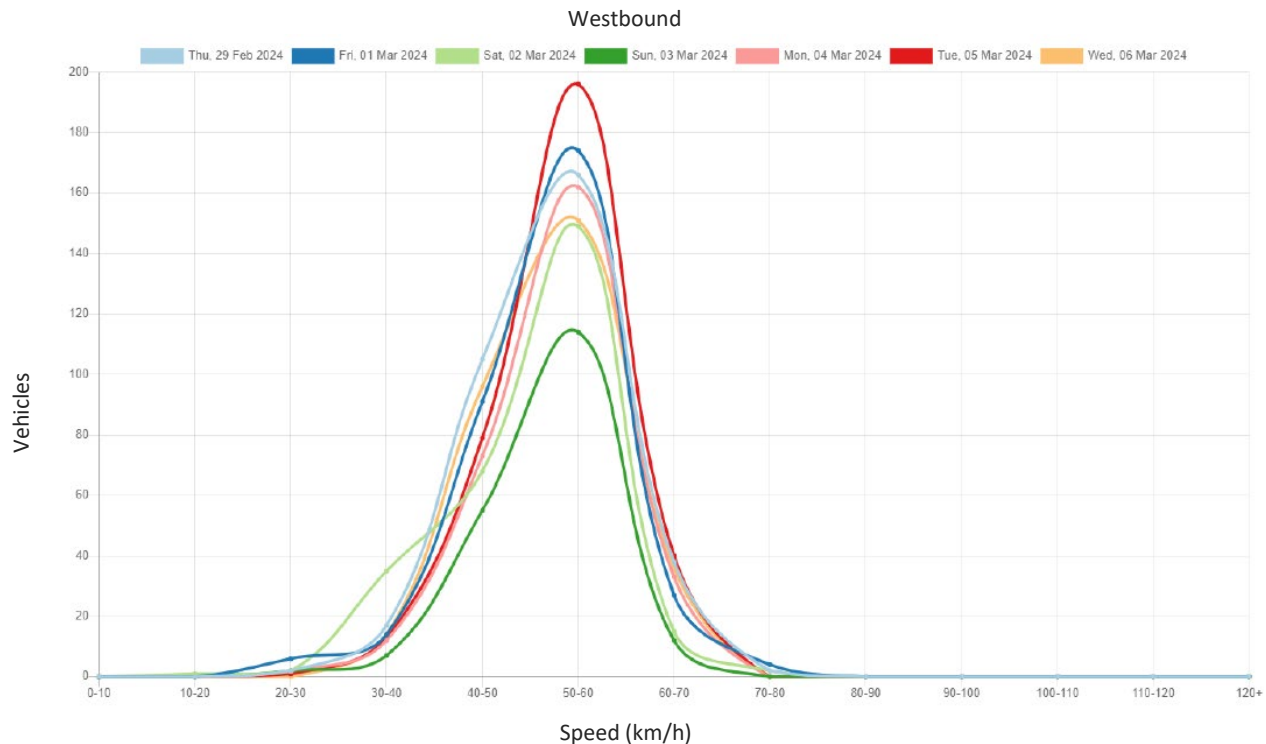


Figure 1-7: Traffic Speed Distribution for Westbound Vehicles

The speed survey indicated that eastbound and westbound vehicles adhere to different speed limits at the survey location. For eastbound traffic, vehicles observe a speed limit of 80km/h (corresponding to an 85% speed of 65.2km/h), while westbound vehicles observe a speed limit of 60km/h (corresponding to an 85% speed of 58.5km/h). Therefore, the higher speed limit of 80km/h would be appropriate to the road as a whole, with a corresponding design speed of 85km/h.

1.2.7.3 Design Speed Conclusion

A design speed assessment was undertaken in accordance with the method set out in TII Rural Road Link Design DN-GEO-03031. The results of the assessment suggest that the design speed for the existing road geometry (over a 2.0km length) is 70km/h which corresponds to a 60km/h speed limit.

A traffic speed survey was conducted approx. 50m to the west of the site entrance between Thursday 29th February and Wednesday 6th March 2024. The observed data shows that the 85th percentile speed for eastbound vehicles was 65.2km/h and for westbound vehicles was 58.5km/h.

Given that the recorded vehicle speeds at the site are higher than the speed limit determined over a 2km length from the speed assessment, a speed limit of 80km/h, and corresponding design speed of 85km/h, will be adopted as the design speed at the site entrance.

The following sections of this report outline a junction visibility assessment at the development access junction and stopping sight distance assessment on Nobber Road for a design speed of 85km/hr. However, a further assessment will be undertaken for a 70km/h design speed in order to assess the development entrance at a lower speed limit, in order to allow an assessment of the merits of lowering the speed limits along this section of Nobber Road.



1.3 Proposed Development

The proposed development site is located at Lough Bracken, approximately 2km southwest of the village of Drumconrath in north Co. Meath. Access to the proposed development will be via the existing junction with Nobber Road.

This project involves enhancement works for Lough Bracken to improve its function as a public amenity and to provide recreational space for new amenity features. Enhancement works include the identification of restorative works which may be necessary to improve the quality of the local environment, ecosystem and infrastructure.

1.4 Junction Visibility

Traffic from a minor road has to join or cross the major road when there are gaps in the major road traffic streams. It is therefore essential that drivers emerging from a minor road shall have adequate visibility in each direction to see the oncoming major road traffic in sufficient time to permit them to make their manoeuvres safely.

The distance back along the minor road from which the full visibility is measured is known as the 'x' distance. It is measured back along the centreline of the minor road or direct access from the continuation of the line of the nearside edge of the paved surface (including hard strip or hard shoulder) of the major road. The desirable minimum 'x' distance is 3.0m.

From the point "x" metres back from the major road a driver approaching the junction along the minor road shall be able to see clearly points to the left and right on the nearer edge of the major road running carriageway at a distance, measured from its intersection with the centreline of the minor road. This is called the 'y' distance and is defined in Table 1-3 below.

Table 1-3: 'y' Visibility Distances from the Minor Road

Design Speed of major road(km/h)	'y' Distance(m)
42	50
50	70
60	90
70	120
85	160
100	215
120	295

An assessment of the existing junction visibility was undertaken using a topographical survey of the junction to confirm the limits of the existing sightlines and the location of obstructions by existing features including trees and vegetation. The assessment was performed utilising an 85km/h design speed relating to the 80km/h posted speed limit. For an 85km/h design speed the 'y' distance is 160m. The assessment was repeated adopting the 70km/h design speed that resulted from the speed limit assessment in Section 1.2.2 of this report. For a 70km/h design speed the 'y' distance is 120m.

The junction visibility drawings for the Lough Bracken junction are shown in Appendix C. Junction visibility lines are shown as red dashed lines. The following drawings are included:

- Extents of Existing Sight Distance
- Sight Distance Requirements for an 85km/h Design Speed
- Sight Distance Requirements for a 70km/h Design Speed



The drawings show that the required 'y' distance of 160m for an 85km/h design speed is not achieved in either direction. The sightlines are blocked by hedgerows and vegetation along the north side of Nobber Road, although to the west of the development junction, the sightline marginally crosses into the northern verge.

For a 70km/h design speed, the required 'y' distance of 120m is achieved on the western approach to the junction however, it is not achieved to the east of the junction.

The achieved junction sight distances are 139m to the west and 101m to the east.

1.5 Stopping Sight Distance

Stopping sight distance is the minimum sight distance required for a driver to identify an unforeseen hazard in the carriageway and have sufficient time to permit them to stop safely. It is therefore essential that drivers shall have adequate stopping sight distance to avoid collisions with potential obstructions.

The stopping sight distance is measured from a driver's eye height between 1.05m and 2.00m, to an object height of between 0.26m and 2.00m above the road surface, creating an envelope of visibility. Measurements are taken along a line in the centre of the traffic lane and checked in the horizontal and vertical planes within the envelope of visibility.

The stopping sight distance is governed primarily by the design speed of the major road and is defined in Table 1-4 below. For an 85km/h design speed the desirable minimum stopping sight distance is 160m. One step below desirable minimum is 120m and two steps below is 90m. For a 70km/h design speed the desirable minimum stopping sight distance is 120m. One step below desirable minimum is 90m and two steps below is 70m.

Table 1-4: Minimum Stopping Sight Distances

DESIGN SPEED (km/h)	120	100	85	70	60	V2/R
STOPPING SIGHT DISTANCE m						
Desirable Minimum Stopping Sight Distance	295	215	160	120	90	
One Step below Desirable Minimum	215	160	120	90	70	
Two Steps below Desirable Minimum	160	120	90	70	50	

Through visibility relates to vehicles on Nobber Road that are not intending to turn into the development and will instead continue past the junction. Drivers in the eastbound lane intending to turn into the development must stop in that lane, opposite the entrance to Lough Bracken. Vehicles travelling in the same lane, in the eastbound direction, must have stopping sight visibility to the stationary vehicle while it is waiting to make its turn. Vehicles travelling in the westbound direction must also have visibility to the turning vehicle as it is about to cross the westbound lane.

An assessment of the existing stopping sight distance was undertaken using the topographical survey of the road to confirm the limits of the existing stopping sight distances and the location of obstructions by existing features including trees and vegetation. The stopping sight distances are shown in the junction visibility drawings in Appendix C.

The through vehicle stopping sight distance along Nobber Road is depicted by blue dashed lines. The desirable minimum stopping sight distance of 160m for an 85km/h design speed is not achieved for either eastbound or westbound vehicles. Similarly, the desirable minimum stopping sight distance of 120m for a 70km/h design speed is not achieved in either direction.



For both design speed scenarios, the sightlines are blocked by the hedgerows along the north side of Nobber Road. The actual sight distance achieved is 41m from the westbound land and 29m from the eastbound lane.

Finally, a vehicle that is stationary on the eastbound lane, intending to turn right into the proposed development, needs to see a sufficient distance ahead to complete the manoeuvre safely. The turning vehicle stopping sight distance is depicted by the pink dashed lines. The achieved sight distance available to a driver in this situation is 41m.

1.6 Potential Upgrade Measures

The existing Nobber Road/Lough Bracken junction will require upgrade works to improve junction safety. Three separate scenarios have been developed as follows:

- Upgrade Measures for an 85km/h Design Speed (80km/h speed limit)
- Upgrade Measures for an 70km/h Design Speed (60km/h speed limit)
- Upgrade Measures - No Land Acquisition (80km/h speed limit)

Drawings showing the proposed upgrade measures for each of the above scenarios are included in Appendix D.

1.6.1 85km/h Design Speed

Full stopping sight distance and junction visibility distances can only be achieved through the acquisition of land along the north side of Nobber Road (inside of the curve). The area of land required is approx. 2,100m² on the eastern side of the development junction and approx. 250m² on the western side. Fencing would need to be provided and the land cleared of trees and vegetation.

The 2,100m² of land on the north side of Nobber Road falls from the north down to the road. This area will likely require earthworks to cut back the existing slope in order to achieve full sight distance. Additional survey would be required to determine ground levels in this area in order allow 3D modelling of the earthworks. This may lead to further land acquisition requirements to account for the cut slope.

A private dwelling approx. 150m north of the development junction will also be affected by the proposed sightline. The front section of the garden would need to be acquired and fenced off.

Additional measures that can be provided include the provision of warning signage and road markings to provide advanced warning to drivers as they approach the junction. This includes provision of worded 'SLOW' markings, and advance warning signage of a junction ahead (W 002L and W 002R) to alert drivers on Nobber Road to the approaching junction.

The existing warning sign for a series of sharp bends (W 053L) on the eastbound approach to the junction will need to be replaced with the appropriate W 053R sign indicating the change in direction of the first bend is on the right side of the road.

The provision of line marking and signage on the access road to Lough Bracken will also be required. This includes stop line marking and worded STOP markings supplemented by a stop sign (RUS 027) and T-junction ahead signage (W 016). Works at the mouth of the junction will require asphalt surfacing to provide a non-granular platform for the stop line markings.



1.6.2 70km/h Design Speed

Similar to the mitigation measures for an 85km/h design speed, full sight distance and junction visibility distances can only be achieved through the acquisition of land along the north side of Nobber Road. Removal of trees and vegetation and provision of new fencing will be required. A reduction in the speed limit to 60km/h would have a corresponding reduction in junction visibility and stopping sight distances from 160m for an 85km/h design speed to 120m for a 70km/h design speed. The resultant land acquisition is less than that required for an 85km/h design speed with approx. 800m² required on the eastern side of the development junction and approx. 120m² required on the western side.

As with the upgrade measures required for an 85km/h design speed, additional survey works will be required to determine the extent of the earthworks on the eastern side of the junction. The earthworks will likely result in additional land required to achieve full sight distance. However, lands would not be required at the private dwelling, north of the development junction.

The provision of road markings and signage is consistent with the proposed measures for an 85km/h design speed outlines in Section 1.6.1.

1.6.3 No Land Acquisition

The measures outlined above to achieve sight distance for an 85km/h or 70km/h design speed require land acquisition along the north side of Nobber Road. Sight distances can also be improved through the removal of trees and/or vegetation along the inside of the curve. Increasing the frequency of hedgerow and verge maintenance opposite the development junction entrance would provide a safer environment in terms of stopping sight distances and junction visibility.

In addition, road markings and signage, consistent with the proposed measures for an 85km/h and 70km/h design speed, could also be implemented.

1.7 Development Access Lay-bys

The access road to Lough Bracken is relatively narrow and typically allows only for one-way traffic movement. The access road contains a long straight approx. 280m in length. There is an existing lay-by at the western end of the straight for eastbound vehicles to pull in and allow westbound vehicles to pass. This lay-by will require upgrade works including asphalt surfacing and provision of linemarking. The existing space available allows for an 18m long, 4m wide lay-by. No additional land is required.

A new lay-by should be provided at the eastern end of the straight section of the access road to enable westbound vehicles to pull in and allow eastbound exiting vehicles to pass. A new lay-by will require the removal of trees and vegetation in addition to land acquisition on the southern side of the access road. Works will include asphalt surfacing and provision of linemarking. Where the layby is constructed over an existing drainage ditch, a new diverted ditch will be constructed around the lay-by. A new fence will need be provided on the southern side of the lay-by where the existing trees/vegetation have been removed. The 4m wide lay-by will provide sufficient space for an 11m long vehicle to pull in.

The drawing showing the proposed layout of the existing and proposed lay-bys is included in Appendix E.



1.8 Conclusion

Based on the assessment undertaken, the ideal scenario is that the proposed mitigation measures for a design speed of 85km/h are implemented for the existing speed limit of 80km/h. This option will have the highest cost due to the amount of land acquisition and earthworks required.

Land acquisition is also required for the provision of full sight distance in a scenario where the speed limit is reduced to 60km/h. However, in this scenario, there are no direct impacts to the dwelling on the north side of Nobber Road.

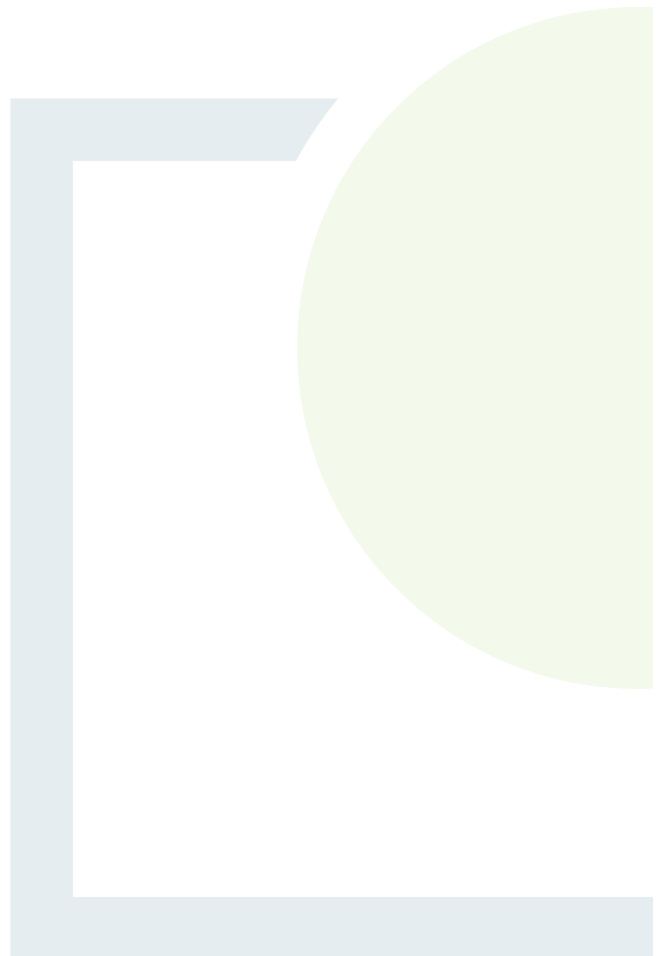
If land acquisition is not viable then the proposed mitigation measures that require no land acquisition are recommended. This will include provision of road markings and signage and an increase in hedgerow maintenance.



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APPENDIX **A**

Design Speed Assessment



9.2.2 Regional and Local Rural Roads less than 2km long:

Where the improved length of Regional or Local Road is less than 2km, the Design Speed shall be derived using the Alignment Constraints (A_c) and Layout Constraints (L_c) measured over a minimum length of 2km. Where a new length of local link road is to be constructed and the length of the new link is less than 300m (e.g. short link roads and cul-de-sacs) an appropriate Design Speed shall be chosen to correspond to the anticipated speed.

Alignment Constraint (A_c)

Clause 9.2.3 of DN-GEO-03031

For Single Carriageway: $A_c = 12 - (VISI/60) + (2B/45)$

B = Bendiness (degrees/km)

135.35

VW = (Average width of verge + hard shoulder (m)

0

VISI = Harmonic Mean Visibility (m)

$\log_{10} VISI = 2.46 + (VW/25) - (B/400)$

$\log_{10} VISI = 2.12125$

$VISI = 132.21$

Therefore,

$A_c = 15.81$

Layout Constraint (L_c)

Clause 9.2.4 of DN-GEO-03031

Junctions and Private Accesses

23

Length (km)

2

Accesses / km

11.5

→

High

→

$L_c = 35$

Carriageway Width (ex. Hard Strips)

approx. average 5.5m

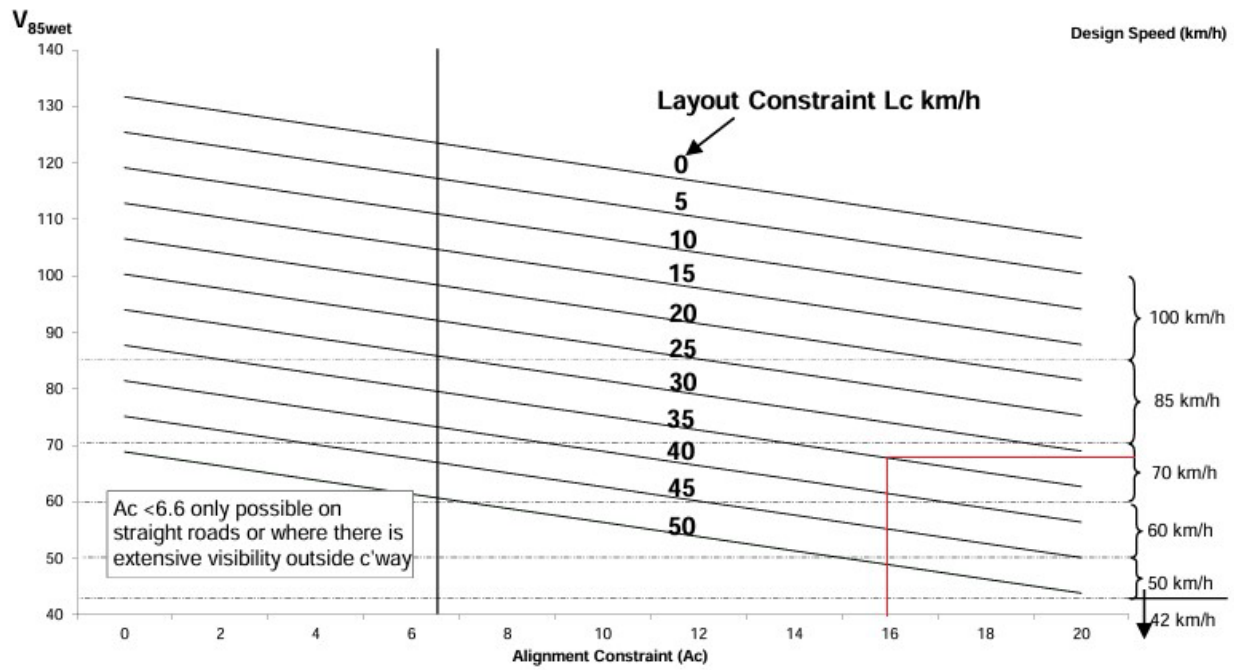
M = Medium Access numbering 6 to 8 per km;

H = High Access numbering 9 or more per km.

Table 9.2 Layout Constraint, L_c km/h

Carriageway width (ex. Hard strips)	3.0m		4.0m		5.0m		6.0m	
Degree of access and junctions	H	M	H	M	H	M	H	M
With 3m verge	47*							
35*	39*	32*	33*	29*	29	26		
With 1.5m verge	49*	37*	41*	34*	35*	31*	31	28
With 0.5m verge	51*	39*	43*	36*	37*	33*	33	30
No verge	53*	41*	45*	38*	39*	35*	35*	32*

*These values were interpolated from Table 1.1 of this Standard



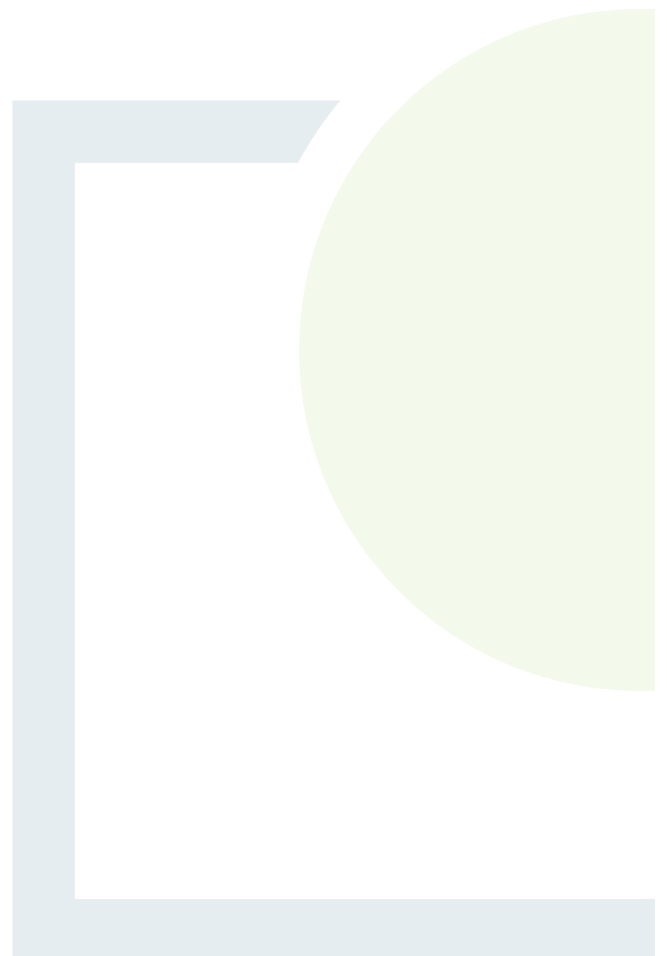
➔ **Design Speed = 70kph**



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APPENDIX **B**

Traffic Survey Data





Data Analysis Services

Traffic-Transportation Commercial-Innovation

24130 - Loughbrackan Co. Meath ATC



Idaso Ltd
National Science Park,
Dublin Road, Mullingar,
Co Westmeath, Ireland



Office
Ph: +353 (0) 4493 18019
Email: info@idaso.ie



www.idaso.ie

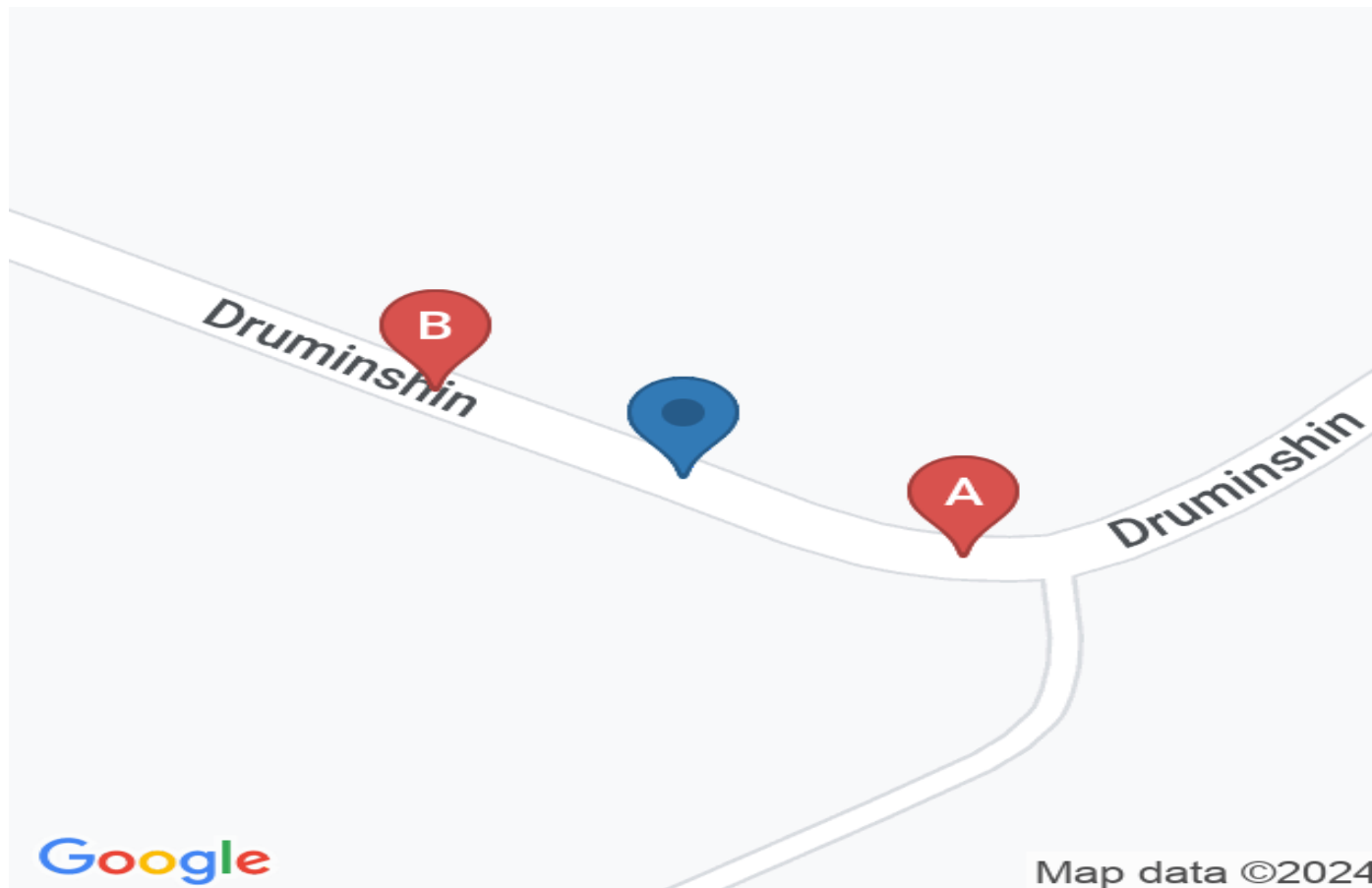
with compliments

Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Druminshin

Date: Thu, 29 Feb 2024 - Wed, 06 Mar 2024



Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Druminshin

Survey Summary

Thu, 29 Feb 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	330	319	649
85% Speed (KPH)	59.0	65.5	62.5
Minimum Speed (KPH)	23.0	19.7	19.7
Maximum Speed (KPH)	75.1	83.4	83.4
Average Speed (KPH)	51.9	56.9	54.4

Fri, 01 Mar 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	316	327	643
85% Speed (KPH)	58.4	65.7	62.3
Minimum Speed (KPH)	22.4	15.0	15.0
Maximum Speed (KPH)	74.2	85.1	85.1
Average Speed (KPH)	51.7	57.1	54.4

Sat, 02 Mar 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	272	273	545
85% Speed (KPH)	57.1	66.4	62.6
Minimum Speed (KPH)	13.3	23.7	13.3
Maximum Speed (KPH)	74.9	86.2	86.2
Average Speed (KPH)	50.1	55.3	52.7

Sun, 03 Mar 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	190	189	379
85% Speed (KPH)	57.3	64.8	62.0
Minimum Speed (KPH)	25.2	24.6	24.6
Maximum Speed (KPH)	66.2	83.3	83.3
Average Speed (KPH)	51.4	56.4	53.9

Mon, 04 Mar 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	282	295	577
85% Speed (KPH)	58.6	63.7	61.8
Minimum Speed (KPH)	22.4	7.2	7.2
Maximum Speed (KPH)	67.2	87.8	87.8
Average Speed (KPH)	52.3	55.9	54.1

Tue, 05 Mar 2024

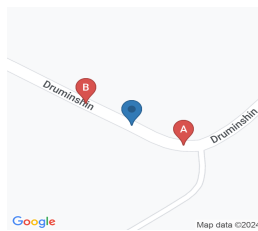
	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	329	331	660
85% Speed (KPH)	59.5	65.2	62.9
Minimum Speed (KPH)	26.8	16.4	16.4
Maximum Speed (KPH)	68.1	79.8	79.8
Average Speed (KPH)	52.8	57.3	55.0

Wed, 06 Mar 2024

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	297	311	608
85% Speed (KPH)	58.7	64.9	63.1
Minimum Speed (KPH)	32.7	22.0	22.0
Maximum Speed (KPH)	68.7	79.1	79.1
Average Speed (KPH)	52.0	56.8	54.5

7 Day Total

	Northbound (A => B)	Southbound (B => A)	Cumulative
No. Vehicles	2016	2045	4061
85% Speed (KPH)	58.5	65.2	62.5
Minimum Speed (KPH)	13.3	7.2	7.2
Maximum Speed (KPH)	75.1	87.8	87.8
Average Speed (KPH)	51.8	56.6	54.2



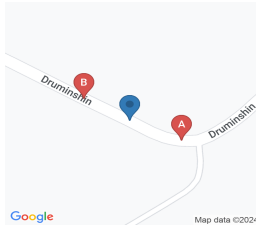
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Thu, 29 Feb 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:45	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
H/TOT	0	1	0	0	0	0	1	1	0	5	0	0	0	0	5	5
6 TOT	0	2	0	0	0	0	2	2	0	5	0	0	0	0	5	5



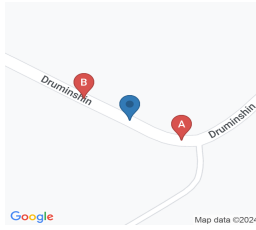
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Thu, 29 Feb 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
06:15	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
06:30	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
06:45	0	1	0	0	0	0	1	1	0	2	1	0	0	0	3	3
H/TOT	0	6	0	0	0	0	6	6	0	5	1	0	0	0	6	6
07:00	0	3	0	0	0	0	3	3	0	0	1	0	1	0	2	3.3
07:15	0	2	0	0	0	0	2	2	0	1	0	1	0	0	2	2.5
07:30	0	5	1	0	0	0	6	6	0	1	0	0	0	0	1	1
07:45	0	5	0	1	0	0	6	6.5	0	7	1	0	0	0	8	8
H/TOT	0	15	1	1	0	0	17	17.5	0	9	2	1	1	0	13	14.8
08:00	0	6	1	0	0	0	7	7	0	4	1	0	0	0	5	5
08:15	0	9	3	0	0	0	12	12	0	6	3	0	0	0	9	9
08:30	0	8	5	1	0	0	14	14.5	0	6	0	2	1	0	9	11.3
08:45	0	5	3	0	0	0	8	8	0	13	1	0	0	0	14	14
H/TOT	0	28	12	1	0	0	41	41.5	0	29	5	2	1	0	37	39.3
09:00	0	1	1	0	0	0	2	2	0	3	2	0	0	0	5	5
09:15	0	1	3	0	0	0	4	4	0	4	1	0	0	0	5	5
09:30	0	3	1	0	0	0	4	4	0	5	1	1	0	0	7	7.5
09:45	0	2	0	0	0	0	2	2	0	3	1	0	1	0	5	6.3
H/TOT	0	7	5	0	0	0	12	12	0	15	5	1	1	0	22	23.8
10:00	0	5	1	0	1	0	7	8.3	0	5	0	0	0	0	5	5
10:15	0	4	0	1	0	0	5	5.5	0	5	0	0	1	0	6	7.3
10:30	0	5	0	0	0	0	5	5	0	3	0	2	0	0	5	6
10:45	0	1	0	0	0	0	1	1	0	6	0	0	0	0	6	6
H/TOT	0	15	1	1	1	0	18	19.8	0	19	0	2	1	0	22	24.3
11:00	0	6	2	0	0	0	8	8	0	9	1	1	0	0	11	11.5
11:15	0	4	0	0	0	0	4	4	0	8	1	0	0	0	9	9
11:30	0	6	1	0	0	0	7	7	0	3	0	0	0	0	3	3
11:45	0	3	1	0	0	0	4	4	0	1	1	0	0	0	2	2
H/TOT	0	19	4	0	0	0	23	23	0	21	3	1	0	0	25	25.5
6 TOT	0	90	23	3	1	0	117	119.8	0	98	16	7	4	0	125	133.7



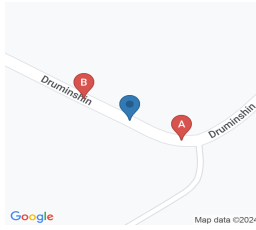
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Thu, 29 Feb 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	6	2	0	0	0	8	8	0	7	2	0	0	0	9	9
12:15	0	4	0	1	0	0	5	5.5	0	2	3	3	0	0	8	9.5
12:30	0	3	1	0	0	0	4	4	0	2	1	0	1	0	4	5.3
12:45	0	5	1	0	0	0	6	6	0	5	1	0	0	0	6	6
H/TOT	0	18	4	1	0	0	23	23.5	0	16	7	3	1	0	27	29.8
13:00	0	8	0	0	0	0	8	8	0	1	2	0	0	0	3	3
13:15	0	10	0	1	0	0	11	11.5	0	3	0	0	0	0	3	3
13:30	0	7	2	0	0	0	9	9	0	4	1	1	0	0	6	6.5
13:45	0	2	2	1	1	0	6	7.8	0	4	1	1	0	0	6	6.5
H/TOT	0	27	4	2	1	0	34	36.3	0	12	4	2	0	0	18	19
14:00	0	3	0	0	0	0	3	3	0	3	1	0	0	0	4	4
14:15	0	7	1	1	0	0	9	9.5	0	5	2	2	0	1	10	12
14:30	0	7	1	2	0	0	10	11	0	3	1	1	0	0	5	5.5
14:45	0	3	2	1	0	0	6	6.5	0	5	2	0	0	0	7	7
H/TOT	0	20	4	4	0	0	28	30	0	16	6	3	0	1	26	28.5
15:00	0	3	2	0	1	0	6	7.3	0	2	3	0	1	0	6	7.3
15:15	0	7	1	0	0	0	8	8	0	2	1	0	0	0	3	3
15:30	0	3	0	0	0	0	3	3	0	4	0	1	0	1	6	7.5
15:45	0	4	1	0	0	0	5	5	0	5	0	0	0	0	5	5
H/TOT	0	17	4	0	1	0	22	23.3	0	13	4	1	1	1	20	22.8
16:00	0	7	1	0	0	0	8	8	0	8	2	1	0	0	11	11.5
16:15	0	3	0	1	0	0	4	4.5	0	5	0	0	0	0	5	5
16:30	0	6	0	0	0	0	6	6	0	6	1	0	0	0	7	7
16:45	0	6	2	1	0	0	9	9.5	0	7	0	0	0	0	7	7
H/TOT	0	22	3	2	0	0	27	28	0	26	3	1	0	0	30	30.5
17:00	0	8	0	0	0	0	8	8	0	6	1	0	1	0	8	9.3
17:15	0	7	0	0	0	0	7	7	0	1	0	1	0	0	2	2.5
17:30	0	6	0	0	0	0	6	6	0	8	2	0	0	0	10	10
17:45	0	3	2	0	0	0	5	5	0	5	0	0	0	1	6	7
H/TOT	0	24	2	0	0	0	26	26	0	20	3	1	1	1	26	28.8
6 TOT	0	128	21	9	2	0	160	167.1	0	103	27	11	3	3	147	159.4



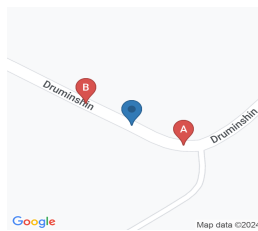
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Thu, 29 Feb 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	4	0	0	0	0	4	4	0	2	1	0	0	0	3	3
18:15	0	5	0	0	0	0	5	5	0	4	1	0	0	0	5	5
18:30	0	3	3	0	0	0	6	6	0	0	0	0	0	0	0	0
18:45	0	5	0	0	0	0	5	5	0	6	1	0	0	0	7	7
H/TOT	0	17	3	0	0	0	20	20	0	12	3	0	0	0	15	15
19:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
19:15	0	0	1	0	0	0	1	1	0	1	1	0	0	0	2	2
19:30	0	1	0	0	0	0	1	1	0	3	1	0	0	0	4	4
19:45	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
H/TOT	0	3	1	0	0	0	4	4	0	6	2	0	0	0	8	8
20:00	0	3	0	0	0	0	3	3	0	0	0	0	0	0	0	0
20:15	0	3	0	0	0	0	3	3	0	1	0	0	0	0	1	1
20:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
20:45	0	2	2	0	0	0	4	4	0	2	1	0	0	0	3	3
H/TOT	0	9	2	0	0	0	11	11	0	4	1	0	0	0	5	5
21:00	0	1	0	0	0	0	1	1	0	4	0	0	0	0	4	4
21:15	0	3	1	0	0	0	4	4	0	0	0	0	0	0	0	0
21:30	0	4	0	0	0	0	4	4	0	3	1	0	0	0	4	4
21:45	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
H/TOT	0	10	1	0	0	0	11	11	0	7	1	0	0	0	8	8
22:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
22:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:30	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
22:45	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
H/TOT	0	4	0	0	0	0	4	4	0	3	0	0	0	0	3	3
23:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
23:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
23:45	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
H/TOT	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
6 TOT	0	44	7	0	0	0	51	51	0	35	7	0	0	0	42	42
24 TOT	0	264	51	12	3	0	330	339.9	0	241	50	18	7	3	319	340.1



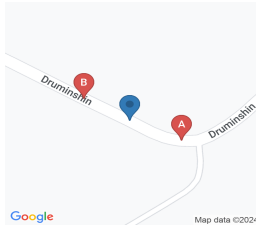
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Fri, 01 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
01:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	1	0	0	1	1.5	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
05:45	0	1	1	0	0	0	2	2	0	0	0	0	0	0	0	0
H/TOT	0	1	1	1	0	0	3	3.5	0	3	0	0	0	0	3	3
6 TOT	0	2	1	1	0	0	4	4.5	0	4	0	0	0	0	4	4



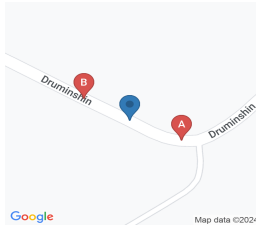
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Fri, 01 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
06:15	0	1	1	0	0	0	2	2	0	2	0	0	0	0	2	2
06:30	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
06:45	0	0	0	0	0	0	0	0	0	3	0	0	0	0	3	3
H/TOT	0	3	1	0	0	0	4	4	0	7	0	0	0	0	7	7
07:00	0	5	0	0	0	0	5	5	0	1	0	0	0	0	1	1
07:15	0	4	0	0	0	0	4	4	0	1	0	1	0	0	2	2.5
07:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
07:45	0	8	0	1	0	0	9	9.5	0	5	1	0	0	0	6	6
H/TOT	0	18	0	1	0	0	19	19.5	0	8	1	1	0	0	10	10.5
08:00	0	3	0	0	0	0	3	3	0	5	2	0	0	0	7	7
08:15	0	11	1	0	1	0	13	14.3	0	5	1	1	0	0	7	7.5
08:30	0	9	2	0	0	0	11	11	0	5	0	2	0	0	7	8
08:45	0	5	2	1	0	0	8	8.5	0	15	1	0	0	0	16	16
H/TOT	0	28	5	1	1	0	35	36.8	0	30	4	3	0	0	37	38.5
09:00	0	3	2	0	0	0	5	5	0	3	1	0	0	0	4	4
09:15	0	0	2	1	0	0	3	3.5	0	8	1	0	0	0	9	9
09:30	0	1	2	0	1	0	4	5.3	0	3	0	0	0	0	3	3
09:45	0	2	0	0	0	0	2	2	0	4	1	0	0	0	5	5
H/TOT	0	6	6	1	1	0	14	15.8	0	18	3	0	0	0	21	21
10:00	0	4	1	0	0	0	5	5	0	3	2	0	0	0	5	5
10:15	0	3	1	0	0	0	4	4	0	0	0	0	0	0	0	0
10:30	0	7	0	0	0	0	7	7	0	4	0	0	0	0	4	4
10:45	0	6	0	0	0	0	6	6	0	3	1	0	0	0	4	4
H/TOT	0	20	2	0	0	0	22	22	0	10	3	0	0	0	13	13
11:00	0	3	0	1	0	0	4	4.5	0	2	3	0	0	0	5	5
11:15	0	9	2	0	0	0	11	11	0	5	1	0	0	0	6	6
11:30	0	5	2	0	0	0	7	7	0	2	2	0	0	0	4	4
11:45	0	4	1	0	0	0	5	5	0	4	0	1	0	0	5	5.5
H/TOT	0	21	5	1	0	0	27	27.5	0	13	6	1	0	0	20	20.5
6 TOT	0	96	19	4	2	0	121	125.6	0	86	17	5	0	0	108	110.5



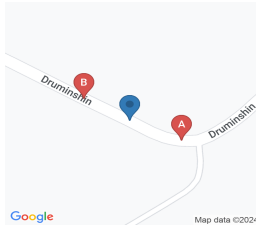
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Fri, 01 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	6	0	0	0	0	6	6	0	1	1	1	0	0	3	3.5
12:15	0	3	1	2	0	0	6	7	0	5	1	0	0	0	6	6
12:30	0	5	1	0	0	0	6	6	0	6	1	0	0	0	7	7
12:45	0	2	2	0	0	0	4	4	0	7	2	0	0	0	9	9
H/TOT	0	16	4	2	0	0	22	23	0	19	5	1	0	0	25	25.5
13:00	0	7	0	0	0	0	7	7	0	3	0	2	0	0	5	6
13:15	0	6	1	0	0	0	7	7	0	4	0	0	0	0	4	4
13:30	0	8	4	0	0	0	12	12	0	12	0	0	0	0	12	12
13:45	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
H/TOT	0	22	5	0	0	0	27	27	0	21	0	2	0	0	23	24
14:00	0	3	1	0	0	0	4	4	0	3	2	0	0	0	5	5
14:15	0	3	0	0	0	0	3	3	0	6	7	0	0	1	14	15
14:30	0	6	1	0	0	0	7	7	0	2	1	0	0	0	3	3
14:45	0	2	3	0	1	0	6	7.3	0	5	2	0	1	0	8	9.3
H/TOT	0	14	5	0	1	0	20	21.3	0	16	12	0	1	1	30	32.3
15:00	0	2	1	0	0	0	3	3	0	6	2	1	0	0	9	9.5
15:15	0	4	1	0	0	0	5	5	0	6	2	0	1	0	9	10.3
15:30	0	6	2	2	0	0	10	11	0	4	2	1	0	1	8	9.5
15:45	0	4	1	0	0	0	5	5	0	7	3	1	0	0	11	11.5
H/TOT	0	16	5	2	0	0	23	24	0	23	9	3	1	1	37	40.8
16:00	0	3	1	0	1	0	5	6.3	0	11	3	1	0	0	15	15.5
16:15	0	6	1	1	1	0	9	10.8	0	6	0	1	0	0	7	7.5
16:30	0	5	0	1	0	0	6	6.5	0	2	0	0	0	0	2	2
16:45	0	3	0	0	1	0	4	5.3	0	5	0	0	0	0	5	5
H/TOT	0	17	2	2	3	0	24	28.9	0	24	3	2	0	0	29	30
17:00	0	7	1	0	0	0	8	8	0	0	2	1	0	0	3	3.5
17:15	0	5	1	0	0	0	6	6	0	2	1	0	0	0	3	3
17:30	0	2	0	0	0	0	2	2	0	4	1	0	0	0	5	5
17:45	0	4	0	0	0	0	4	4	0	2	0	0	0	0	2	2
H/TOT	0	18	2	0	0	0	20	20	0	8	4	1	0	0	13	13.5
6 TOT	0	103	23	6	4	0	136	144.2	0	111	33	9	2	2	157	166.1



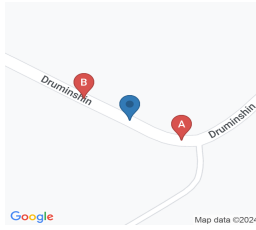
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Fri, 01 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	5	0	0	2	0	7	9.6	0	7	0	1	0	0	8	8.5
18:15	0	1	1	0	0	0	2	2	0	1	1	0	0	0	2	2
18:30	0	4	1	0	0	0	5	5	1	2	0	0	0	0	3	2.4
18:45	0	3	0	0	0	0	3	3	0	7	1	0	0	0	8	8
H/TOT	0	13	2	0	2	0	17	19.6	1	17	2	1	0	0	21	20.9
19:00	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
19:15	0	2	0	0	0	0	2	2	0	4	0	0	0	0	4	4
19:30	0	6	0	1	0	0	7	7.5	0	2	0	0	0	0	2	2
19:45	0	4	0	0	0	0	4	4	0	2	0	0	0	0	2	2
H/TOT	0	13	0	1	0	0	14	14.5	0	9	0	0	0	0	9	9
20:00	0	4	0	0	0	0	4	4	0	2	2	0	0	0	4	4
20:15	0	1	1	0	0	0	2	2	0	4	2	0	0	0	6	6
20:30	0	2	0	0	0	0	2	2	0	7	1	0	0	0	8	8
20:45	0	3	0	0	0	0	3	3	0	0	1	0	0	0	1	1
H/TOT	0	10	1	0	0	0	11	11	0	13	6	0	0	0	19	19
21:00	0	3	1	0	0	0	4	4	0	1	0	0	0	0	1	1
21:15	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
21:30	0	0	0	0	0	0	0	0	0	2	1	0	0	0	3	3
21:45	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
H/TOT	0	5	1	0	0	0	6	6	0	4	1	0	0	0	5	5
22:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:15	0	2	1	0	0	0	3	3	0	1	0	0	0	0	1	1
22:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	2	1	0	0	0	3	3	0	1	0	0	0	0	1	1
23:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
23:15	0	0	1	0	0	0	1	1	0	0	0	0	0	0	0	0
23:30	0	3	0	0	0	0	3	3	0	1	1	0	0	0	2	2
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	3	1	0	0	0	4	4	0	2	1	0	0	0	3	3
6 TOT	0	46	6	1	2	0	55	58.1	1	46	10	1	0	0	58	57.9
24 TOT	0	247	49	12	8	0	316	332.4	1	247	60	15	2	2	327	338.5



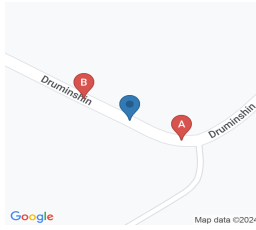
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sat, 02 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
00:15	0	0	0	0	0	0	0	0	0	3	0	0	0	0	3	3
00:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
00:45	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
H/TOT	0	2	0	0	0	0	2	2	0	5	0	0	0	0	5	5
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
01:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
01:45	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
H/TOT	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 TOT	0	3	0	0	0	0	3	3	0	7	0	0	0	0	7	7



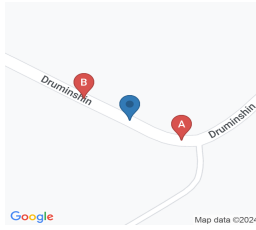
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sat, 02 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
06:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	0	1	0	0	0	1	1
07:00	0	2	0	0	0	0	2	2	0	3	0	0	0	0	3	3
07:15	0	2	0	0	0	0	2	2	0	0	1	0	0	0	1	1
07:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
07:45	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
H/TOT	0	5	0	0	0	0	5	5	0	5	1	0	0	0	6	6
08:00	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
08:15	0	1	0	0	0	0	1	1	0	0	0	1	0	0	1	1.5
08:30	0	3	0	1	0	0	4	4.5	0	3	0	0	0	0	3	3
08:45	0	2	0	1	0	0	3	3.5	0	3	1	1	0	0	5	5.5
H/TOT	0	7	0	2	0	0	9	10	0	9	1	2	0	0	12	13
09:00	0	5	1	0	1	0	7	8.3	0	1	1	0	0	0	2	2
09:15	0	1	0	0	0	0	1	1	0	4	0	0	0	0	4	4
09:30	0	6	1	0	0	0	7	7	0	3	0	1	0	0	4	4.5
09:45	0	0	1	0	0	0	1	1	0	2	1	0	1	0	4	5.3
H/TOT	0	12	3	0	1	0	16	17.3	0	10	2	1	1	0	14	15.8
10:00	0	2	0	0	0	0	2	2	0	4	0	0	1	0	5	6.3
10:15	0	6	0	0	0	0	6	6	0	5	2	0	0	0	7	7
10:30	0	0	0	0	0	0	0	0	0	2	1	0	1	0	4	5.3
10:45	0	6	1	0	1	0	8	9.3	0	2	0	0	2	0	4	6.6
H/TOT	0	14	1	0	1	0	16	17.3	0	13	3	0	4	0	20	25.2
11:00	0	4	1	0	2	0	7	9.6	0	8	0	0	1	0	9	10.3
11:15	0	5	1	0	0	0	6	6	0	5	2	0	2	0	9	11.6
11:30	0	7	3	0	0	0	10	10	1	8	1	0	1	0	11	11.7
11:45	0	5	0	0	1	0	6	7.3	0	3	0	0	1	0	4	5.3
H/TOT	0	21	5	0	3	0	29	32.9	1	24	3	0	5	0	33	38.9
6 TOT	0	60	9	2	5	0	76	83.5	1	61	11	3	10	0	86	99.9



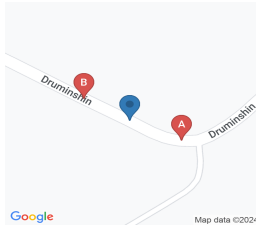
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sat, 02 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	8	1	0	0	0	9	9	0	5	0	1	1	0	7	8.8
12:15	0	5	0	0	1	0	6	7.3	0	4	1	0	0	0	5	5
12:30	0	7	0	1	0	0	8	8.5	0	2	3	0	1	0	6	7.3
12:45	0	3	1	0	0	0	4	4	0	2	1	0	1	0	4	5.3
H/TOT	0	23	2	1	1	0	27	28.8	0	13	5	1	3	0	22	26.4
13:00	0	4	1	0	2	0	7	9.6	0	4	0	0	0	0	4	4
13:15	0	6	1	0	1	0	8	9.3	0	4	1	0	1	0	6	7.3
13:30	0	2	1	0	0	0	3	3	0	4	0	0	1	0	5	6.3
13:45	0	3	0	0	0	0	3	3	0	7	0	0	0	0	7	7
H/TOT	0	15	3	0	3	0	21	24.9	0	19	1	0	2	0	22	24.6
14:00	0	2	0	1	0	0	3	3.5	0	3	0	0	0	0	3	3
14:15	0	4	1	0	0	0	5	5	0	4	1	0	0	0	5	5
14:30	0	10	0	0	0	0	10	10	0	9	1	0	0	0	10	10
14:45	0	2	1	0	0	0	3	3	0	1	0	0	1	0	2	3.3
H/TOT	0	18	2	1	0	0	21	21.5	0	17	2	0	1	0	20	21.3
15:00	0	3	0	0	0	0	3	3	0	4	1	0	1	0	6	7.3
15:15	0	4	0	0	0	0	4	4	0	5	2	0	0	0	7	7
15:30	0	6	0	0	0	0	6	6	0	7	0	0	1	0	8	9.3
15:45	0	3	1	1	0	0	5	5.5	0	2	1	0	0	0	3	3
H/TOT	0	16	1	1	0	0	18	18.5	0	18	4	0	2	0	24	26.6
16:00	0	6	2	0	1	0	9	10.3	0	3	3	0	1	0	7	8.3
16:15	0	5	2	0	0	0	7	7	0	3	1	0	1	0	5	6.3
16:30	0	9	1	0	0	0	10	10	0	8	1	0	0	0	9	9
16:45	0	4	0	0	0	0	4	4	0	2	0	0	1	0	3	4.3
H/TOT	0	24	5	0	1	0	30	31.3	0	16	5	0	3	0	24	27.9
17:00	0	3	2	0	0	0	5	5	0	2	0	0	1	0	3	4.3
17:15	0	3	1	0	0	0	4	4	0	3	0	0	0	0	3	3
17:30	0	7	0	0	0	0	7	7	0	1	1	0	2	0	4	6.6
17:45	0	1	1	0	0	0	2	2	0	5	2	0	0	0	7	7
H/TOT	0	14	4	0	0	0	18	18	0	11	3	0	3	0	17	20.9
6 TOT	0	110	17	3	5	0	135	143	0	94	20	1	14	0	129	147.7



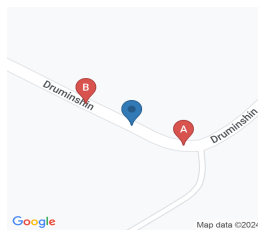
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sat, 02 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	10	0	0	0	0	10	10	0	8	1	0	0	0	9	9
18:15	0	5	2	0	0	0	7	7	0	7	1	0	0	0	8	8
18:30	0	2	0	0	0	0	2	2	0	3	1	0	0	0	4	4
18:45	0	3	0	0	0	0	3	3	0	4	1	0	1	0	6	7.3
H/TOT	0	20	2	0	0	0	22	22	0	22	4	0	1	0	27	28.3
19:00	0	2	0	0	0	0	2	2	0	2	0	0	0	0	2	2
19:15	0	3	0	0	0	0	3	3	0	4	0	0	0	0	4	4
19:30	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
19:45	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
H/TOT	0	8	0	0	0	0	8	8	0	9	0	0	0	0	9	9
20:00	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
20:15	0	15	0	0	0	0	15	15	0	1	0	0	0	0	1	1
20:30	0	4	0	0	0	0	4	4	0	1	0	0	0	0	1	1
20:45	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	2
H/TOT	0	21	0	0	0	0	21	21	0	2	2	0	0	0	4	4
21:00	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
21:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
21:30	0	3	0	0	0	0	3	3	0	0	0	0	0	0	0	0
21:45	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
H/TOT	0	5	0	0	0	0	5	5	0	2	0	0	0	0	2	2
22:00	0	0	0	0	0	0	0	0	0	3	0	0	0	0	3	3
22:15	0	2	0	0	0	0	2	2	0	3	0	0	0	0	3	3
22:30	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
22:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	2	0	0	0	0	2	2	0	8	0	0	0	0	8	8
23:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
6 TOT	0	56	2	0	0	0	58	58	0	44	6	0	1	0	51	52.3
24 TOT	0	229	28	5	10	0	272	287.5	1	206	37	4	25	0	273	306.9



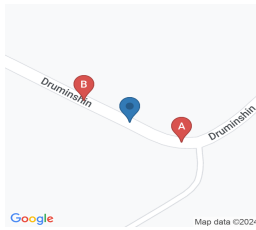
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sun, 03 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	1	1	0	0	0	2	2	0	0	0	0	0	0	0	0
00:15	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
00:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	2	1	0	0	0	3	3	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
01:45	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
02:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	2	0	0	0	0	2	2	0	2	0	0	0	0	2	2
6 TOT	0	6	1	0	0	0	7	7	0	6	0	0	0	0	6	6



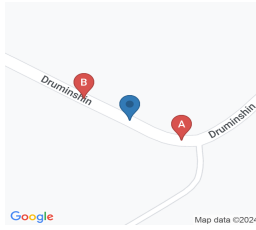
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sun, 03 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
H/TOT	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
07:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
07:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
08:00	0	0	0	0	0	0	0	0	0	2	2	0	0	0	4	4
08:15	0	4	0	0	0	0	4	4	0	0	0	0	0	0	0	0
08:30	0	0	0	0	1	0	1	2.3	0	1	0	0	0	0	1	1
08:45	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
H/TOT	0	5	0	0	1	0	6	7.3	0	5	2	0	0	0	7	7
09:00	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
09:15	0	3	0	0	0	0	3	3	0	0	1	0	0	0	1	1
09:30	0	2	0	0	0	0	2	2	0	3	1	0	0	0	4	4
09:45	0	4	0	0	0	0	4	4	0	0	0	0	0	0	0	0
H/TOT	0	10	0	0	0	0	10	10	0	4	2	0	0	0	6	6
10:00	0	2	0	0	0	0	2	2	0	2	1	0	0	0	3	3
10:15	0	1	1	0	0	0	2	2	0	0	0	0	0	0	0	0
10:30	0	1	0	0	0	0	1	1	0	1	2	0	0	0	3	3
10:45	0	5	1	0	0	0	6	6	0	5	0	0	0	0	5	5
H/TOT	0	9	2	0	0	0	11	11	0	8	3	0	0	0	11	11
11:00	0	2	0	0	0	0	2	2	0	5	1	0	0	0	6	6
11:15	0	1	0	0	0	0	1	1	0	2	1	0	2	0	5	7.6
11:30	0	2	2	0	0	0	4	4	0	2	0	0	0	0	2	2
11:45	0	0	0	0	0	0	0	0	0	4	1	0	0	0	5	5
H/TOT	0	5	2	0	0	0	7	7	0	13	3	0	2	0	18	20.6
6 TOT	0	30	4	0	1	0	35	36.3	0	32	10	0	2	0	44	46.6



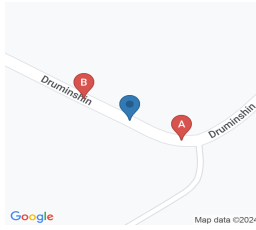
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sun, 03 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	3	0	0	0	0	3	3	0	2	0	0	0	0	2	2
12:15	0	5	0	0	0	0	5	5	0	2	1	0	0	0	3	3
12:30	0	5	2	0	0	0	7	7	0	2	0	0	0	0	2	2
12:45	0	2	1	0	0	0	3	3	0	4	3	0	1	0	8	9.3
H/TOT	0	15	3	0	0	0	18	18	0	10	4	0	1	0	15	16.3
13:00	0	3	0	0	0	0	3	3	0	2	0	0	0	0	2	2
13:15	0	2	2	0	0	0	4	4	0	6	1	1	0	0	8	8.5
13:30	0	6	3	0	0	0	9	9	0	8	1	0	0	0	9	9
13:45	0	6	0	0	0	0	6	6	0	5	1	1	0	0	7	7.5
H/TOT	0	17	5	0	0	0	22	22	0	21	3	2	0	0	26	27
14:00	0	3	0	0	0	0	3	3	0	5	1	0	0	0	6	6
14:15	0	5	1	0	0	0	6	6	0	4	2	0	0	0	6	6
14:30	0	3	1	0	0	0	4	4	0	6	1	0	0	0	7	7
14:45	0	3	0	0	0	0	3	3	3	3	0	0	0	0	6	4.2
H/TOT	0	14	2	0	0	0	16	16	3	18	4	0	0	0	25	23.2
15:00	0	5	0	0	0	0	5	5	0	7	0	1	0	0	8	8.5
15:15	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
15:30	0	2	0	0	0	0	2	2	0	6	0	0	0	0	6	6
15:45	1	6	0	0	0	0	7	6.4	0	3	0	0	0	0	3	3
H/TOT	1	14	0	0	0	0	15	14.4	0	17	0	1	0	0	18	18.5
16:00	0	4	1	0	0	0	5	5	0	2	1	0	0	0	3	3
16:15	0	6	0	0	0	0	6	6	0	2	0	0	0	0	2	2
16:30	0	5	0	0	0	0	5	5	0	2	1	0	0	0	3	3
16:45	0	3	0	0	0	0	3	3	0	1	1	0	0	0	2	2
H/TOT	0	18	1	0	0	0	19	19	0	7	3	0	0	0	10	10
17:00	0	2	0	0	0	0	2	2	0	3	0	0	0	0	3	3
17:15	0	1	1	0	0	0	2	2	0	2	2	0	0	0	4	4
17:30	0	7	0	1	0	0	8	8.5	0	1	0	1	0	0	2	2.5
17:45	0	3	0	0	0	0	3	3	0	1	1	0	0	0	2	2
H/TOT	0	13	1	1	0	0	15	15.5	0	7	3	1	0	0	11	11.5
6 TOT	1	91	12	1	0	0	105	104.9	3	80	17	4	1	0	105	106.5



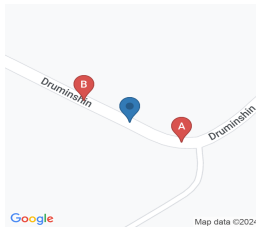
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Sun, 03 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	3	1	0	0	0	4	4	0	4	0	0	0	0	4	4
18:15	0	1	0	0	0	0	1	1	0	4	0	0	0	0	4	4
18:30	0	3	0	0	0	0	3	3	0	4	1	1	0	0	6	6.5
18:45	0	5	1	0	0	0	6	6	0	4	0	0	0	0	4	4
H/TOT	0	12	2	0	0	0	14	14	0	16	1	1	0	0	18	18.5
19:00	0	1	0	0	0	0	1	1	0	2	1	0	0	0	3	3
19:15	0	4	0	0	0	0	4	4	0	4	1	0	0	0	5	5
19:30	0	5	0	0	0	0	5	5	0	0	0	0	0	0	0	0
19:45	0	3	0	1	0	0	4	4.5	0	0	0	0	0	0	0	0
H/TOT	0	13	0	1	0	0	14	14.5	0	6	2	0	0	0	8	8
20:00	0	0	1	0	0	0	1	1	0	1	1	0	0	0	2	2
20:15	0	3	0	0	0	0	3	3	0	0	1	0	0	0	1	1
20:30	0	1	2	0	0	0	3	3	0	0	0	0	0	0	0	0
20:45	0	1	1	0	0	0	2	2	0	3	0	0	0	0	3	3
H/TOT	0	5	4	0	0	0	9	9	0	4	2	0	0	0	6	6
21:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
21:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
21:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
21:45	0	4	0	0	0	0	4	4	0	0	0	0	0	0	0	0
H/TOT	0	5	0	0	0	0	5	5	0	0	0	0	0	0	0	0
22:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:30	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
22:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
23:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 TOT	0	36	6	1	0	0	43	43.5	0	28	5	1	0	0	34	34.5
24 TOT	1	163	23	2	1	0	190	191.7	3	146	32	5	3	0	189	193.6



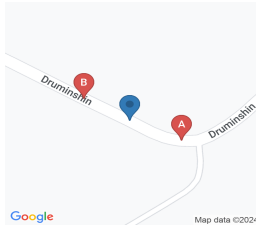
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Mon, 04 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
H/TOT	0	1	0	0	0	0	1	1	0	1	1	0	0	0	2	2
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
05:45	0	0	0	1	0	0	1	1.5	0	0	0	0	0	0	0	0
H/TOT	0	0	0	1	0	0	1	1.5	0	3	0	0	0	0	3	3
6 TOT	0	2	0	1	0	0	3	3.5	0	4	2	0	0	0	6	6



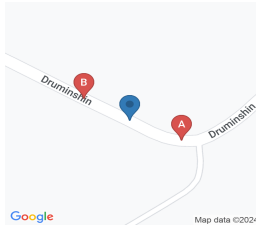
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Mon, 04 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
06:15	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
06:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
06:45	0	1	0	0	0	0	1	1	0	2	1	0	0	0	3	3
H/TOT	0	4	0	0	0	0	4	4	0	4	1	0	0	0	5	5
07:00	0	6	0	0	0	0	6	6	0	2	0	0	0	0	2	2
07:15	0	3	0	0	0	0	3	3	0	2	2	1	0	0	5	5.5
07:30	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
07:45	0	4	1	0	0	0	5	5	0	5	0	0	0	0	5	5
H/TOT	0	14	1	0	0	0	15	15	0	12	2	1	0	0	15	15.5
08:00	0	3	1	0	0	0	4	4	0	2	3	0	0	0	5	5
08:15	0	7	2	0	0	0	9	9	0	3	0	0	0	0	3	3
08:30	0	2	1	0	0	0	3	3	0	1	0	0	0	0	1	1
08:45	0	8	3	0	0	0	11	11	0	9	2	0	0	0	11	11
H/TOT	0	20	7	0	0	0	27	27	0	15	5	0	0	0	20	20
09:00	0	4	1	1	0	0	6	6.5	0	11	1	1	0	0	13	13.5
09:15	0	2	1	0	0	0	3	3	0	3	1	0	0	0	4	4
09:30	0	5	2	1	0	0	8	8.5	0	7	1	0	0	1	9	10
09:45	0	0	0	0	0	0	0	0	0	3	0	1	0	0	4	4.5
H/TOT	0	11	4	2	0	0	17	18	0	24	3	2	0	1	30	32
10:00	0	3	1	1	0	0	5	5.5	0	3	0	0	0	0	3	3
10:15	0	2	1	0	0	0	3	3	0	3	1	0	1	0	5	6.3
10:30	0	5	2	1	0	1	9	10.5	0	5	1	0	0	0	6	6
10:45	0	3	2	0	0	0	5	5	0	1	0	0	0	0	1	1
H/TOT	0	13	6	2	0	1	22	24	0	12	2	0	1	0	15	16.3
11:00	0	7	0	0	0	0	7	7	0	4	1	1	0	0	6	6.5
11:15	0	2	0	0	0	0	2	2	0	3	0	0	0	0	3	3
11:30	0	0	1	0	0	0	1	1	0	4	0	0	0	0	4	4
11:45	0	9	1	1	0	0	11	11.5	0	7	0	1	0	0	8	8.5
H/TOT	0	18	2	1	0	0	21	21.5	0	18	1	2	0	0	21	22
6 TOT	0	80	20	5	0	1	106	109.5	0	85	14	5	1	1	106	110.8



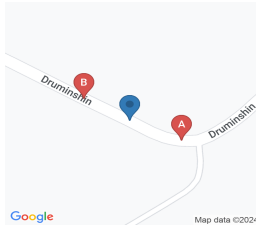
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Mon, 04 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	6	0	0	0	0	6	6	0	3	1	0	0	0	4	4
12:15	0	1	2	0	0	0	3	3	0	6	1	0	0	0	7	7
12:30	0	8	1	0	0	0	9	9	0	3	0	0	0	0	3	3
12:45	0	4	0	0	0	0	4	4	0	5	3	0	1	0	9	10.3
H/TOT	0	19	3	0	0	0	22	22	0	17	5	0	1	0	23	24.3
13:00	0	3	0	0	0	0	3	3	0	3	1	1	0	0	5	5.5
13:15	0	4	1	0	0	0	5	5	0	6	1	0	0	0	7	7
13:30	0	4	2	0	0	0	6	6	0	0	1	0	0	0	1	1
13:45	0	0	0	0	0	0	0	0	0	2	2	1	0	0	5	5.5
H/TOT	0	11	3	0	0	0	14	14	0	11	5	2	0	0	18	19
14:00	0	4	1	0	0	0	5	5	0	4	2	0	0	1	7	8
14:15	0	2	0	0	0	0	2	2	0	9	0	0	0	0	9	9
14:30	0	7	1	0	0	0	8	8	0	4	1	0	0	0	5	5
14:45	0	5	2	0	0	0	7	7	0	7	2	1	0	0	10	10.5
H/TOT	0	18	4	0	0	0	22	22	0	24	5	1	0	1	31	32.5
15:00	0	2	1	0	0	0	3	3	0	2	1	1	0	0	4	4.5
15:15	0	2	1	0	1	0	4	5.3	0	3	0	0	1	0	4	5.3
15:30	0	4	2	1	0	0	7	7.5	0	3	2	1	0	0	6	6.5
15:45	0	0	1	0	0	0	1	1	0	1	1	0	1	0	3	4.3
H/TOT	0	8	5	1	1	0	15	16.8	0	9	4	2	2	0	17	20.6
16:00	0	10	1	0	0	0	11	11	0	4	1	0	0	0	5	5
16:15	0	7	1	0	0	0	8	8	0	2	0	1	0	0	3	3.5
16:30	0	5	1	0	0	0	6	6	0	4	0	0	0	0	4	4
16:45	0	1	0	1	0	0	2	2.5	0	2	1	1	0	0	4	4.5
H/TOT	0	23	3	1	0	0	27	27.5	0	12	2	2	0	0	16	17
17:00	0	3	0	0	0	0	3	3	0	8	3	0	0	0	11	11
17:15	0	4	0	0	0	0	4	4	0	5	0	0	0	0	5	5
17:30	0	4	1	0	0	0	5	5	0	1	2	0	0	0	3	3
17:45	0	5	1	0	0	0	6	6	0	8	1	0	0	0	9	9
H/TOT	0	16	2	0	0	0	18	18	0	22	6	0	0	0	28	28
6 TOT	0	95	20	2	1	0	118	120.3	0	95	27	7	3	1	133	141.4



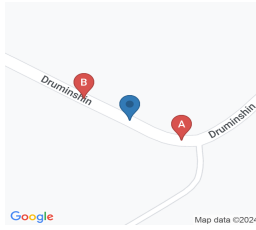
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Mon, 04 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	4	0	0	0	0	4	4	0	2	0	0	0	0	2	2
18:15	0	4	0	0	0	0	4	4	0	6	0	0	0	0	6	6
18:30	0	9	0	0	0	0	9	9	0	3	2	0	0	0	5	5
18:45	0	1	1	0	0	0	2	2	0	3	0	0	0	0	3	3
H/TOT	0	18	1	0	0	0	19	19	0	14	2	0	0	0	16	16
19:00	0	1	1	0	0	0	2	2	0	4	0	0	0	0	4	4
19:15	0	4	1	0	0	0	5	5	0	1	2	0	0	0	3	3
19:30	0	4	1	0	0	0	5	5	0	2	2	0	0	0	4	4
19:45	0	0	1	0	0	0	1	1	0	5	0	0	0	0	5	5
H/TOT	0	9	4	0	0	0	13	13	0	12	4	0	0	0	16	16
20:00	0	2	1	0	0	0	3	3	0	1	1	0	0	0	2	2
20:15	0	4	0	0	0	0	4	4	0	0	1	0	0	0	1	1
20:30	0	5	0	0	0	0	5	5	0	4	0	0	0	0	4	4
20:45	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
H/TOT	0	11	1	0	0	0	12	12	0	6	2	0	0	0	8	8
21:00	0	3	0	0	0	0	3	3	0	3	0	0	0	0	3	3
21:15	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
21:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
21:45	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
H/TOT	0	6	0	0	0	0	6	6	0	6	0	0	0	0	6	6
22:00	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
22:15	0	1	0	0	0	0	1	1	0	0	1	0	0	0	1	1
22:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
22:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	3	0	0	0	0	3	3	0	1	1	0	0	0	2	2
23:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
23:15	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
23:30	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	2	0	0	0	0	2	2	0	2	0	0	0	0	2	2
6 TOT	0	49	6	0	0	0	55	55	0	41	9	0	0	0	50	50
24 TOT	0	226	46	8	1	1	282	288.3	0	225	52	12	4	2	295	308.2



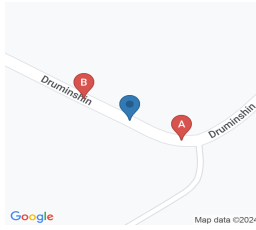
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Tue, 05 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
00:15	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
00:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	3	0	0	0	0	3	3	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
H/TOT	0	0	0	0	0	0	0	0	0	3	0	0	0	0	3	3
6 TOT	0	4	0	0	0	0	4	4	0	3	1	0	0	0	4	4



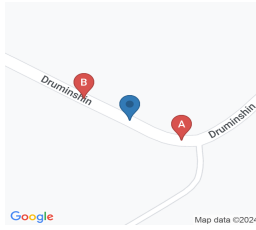
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Tue, 05 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	0	2	0	0	0	2	2	0	3	0	0	0	0	3	3
06:15	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
06:30	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
06:45	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
H/TOT	0	4	2	0	0	0	6	6	0	9	0	0	0	0	9	9
07:00	0	3	0	0	0	0	3	3	0	0	0	0	0	0	0	0
07:15	0	4	0	0	0	0	4	4	0	5	1	1	0	0	7	7.5
07:30	0	1	1	0	0	0	2	2	0	4	0	1	0	0	5	5.5
07:45	0	6	0	2	0	0	8	9	0	2	0	0	0	0	2	2
H/TOT	0	14	1	2	0	0	17	18	0	11	1	2	0	0	14	15
08:00	0	5	0	0	0	0	5	5	0	7	1	0	0	0	8	8
08:15	0	9	2	0	0	0	11	11	0	5	1	0	0	0	6	6
08:30	0	7	4	0	0	0	11	11	0	7	4	0	0	0	11	11
08:45	0	7	2	1	0	0	10	10.5	0	13	0	0	0	0	13	13
H/TOT	0	28	8	1	0	0	37	37.5	0	32	6	0	0	0	38	38
09:00	0	2	0	1	0	0	3	3.5	0	1	1	0	0	0	2	2
09:15	0	1	1	1	0	0	3	3.5	0	3	1	0	0	0	4	4
09:30	0	5	1	1	0	0	7	7.5	0	3	0	0	0	0	3	3
09:45	0	4	1	0	0	0	5	5	0	3	0	0	0	0	3	3
H/TOT	0	12	3	3	0	0	18	19.5	0	10	2	0	0	0	12	12
10:00	0	1	0	0	0	0	1	1	0	2	0	0	0	0	2	2
10:15	0	4	1	0	0	0	5	5	0	3	0	0	0	0	3	3
10:30	0	2	2	1	0	0	5	5.5	0	2	1	0	2	0	5	7.6
10:45	0	1	0	0	0	0	1	1	0	6	0	0	0	0	6	6
H/TOT	0	8	3	1	0	0	12	12.5	0	13	1	0	2	0	16	18.6
11:00	0	4	0	0	0	0	4	4	0	6	0	0	0	0	6	6
11:15	0	1	1	1	0	0	3	3.5	0	3	1	0	0	0	4	4
11:30	0	2	0	1	0	0	3	3.5	0	7	1	1	0	0	9	9.5
11:45	0	3	1	0	0	0	4	4	0	4	1	0	0	0	5	5
H/TOT	0	10	2	2	0	0	14	15	0	20	3	1	0	0	24	24.5
6 TOT	0	76	19	9	0	0	104	108.5	0	95	13	3	2	0	113	117.1



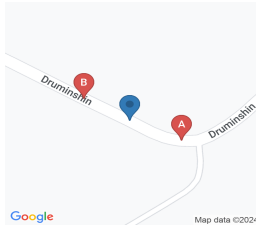
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Tue, 05 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	2	1	0	0	0	3	3	0	2	1	0	0	0	3	3
12:15	0	4	0	0	0	0	4	4	0	2	1	1	0	0	4	4.5
12:30	0	4	1	0	1	0	6	7.3	0	1	0	0	1	0	2	3.3
12:45	0	5	1	0	0	0	6	6	0	6	1	0	0	0	7	7
H/TOT	0	15	3	0	1	0	19	20.3	0	11	3	1	1	0	16	17.8
13:00	0	4	0	0	0	0	4	4	0	3	0	0	0	0	3	3
13:15	0	5	0	0	0	0	5	5	0	3	0	1	0	0	4	4.5
13:30	0	3	1	0	0	0	4	4	0	2	0	0	0	0	2	2
13:45	0	5	0	1	0	0	6	6.5	0	3	1	0	0	0	4	4
H/TOT	0	17	1	1	0	0	19	19.5	0	11	1	1	0	0	13	13.5
14:00	0	3	0	0	0	0	3	3	0	7	1	0	0	0	8	8
14:15	0	3	0	0	0	0	3	3	0	6	2	1	0	1	10	11.5
14:30	0	8	2	0	0	0	10	10	0	6	1	0	0	0	7	7
14:45	0	8	0	0	0	0	8	8	0	6	1	0	1	0	8	9.3
H/TOT	0	22	2	0	0	0	24	24	0	25	5	1	1	1	33	35.8
15:00	0	6	0	0	0	0	6	6	0	6	2	1	0	0	9	9.5
15:15	0	7	2	0	0	0	9	9	0	3	3	0	1	0	7	8.3
15:30	0	4	1	0	0	0	5	5	0	4	1	0	0	0	5	5
15:45	0	5	3	0	0	0	8	8	0	3	1	0	1	0	5	6.3
H/TOT	0	22	6	0	0	0	28	28	0	16	7	1	2	0	26	29.1
16:00	0	3	1	0	0	0	4	4	0	4	1	0	0	0	5	5
16:15	0	8	1	0	0	0	9	9	0	8	1	1	0	1	11	12.5
16:30	0	4	2	0	0	0	6	6	0	4	3	0	0	0	7	7
16:45	0	10	0	0	0	0	10	10	0	8	1	0	0	0	9	9
H/TOT	0	25	4	0	0	0	29	29	0	24	6	1	0	1	32	33.5
17:00	0	6	0	0	0	0	6	6	0	9	6	0	0	0	15	15
17:15	0	9	0	0	0	0	9	9	0	6	0	1	0	0	7	7.5
17:30	0	4	3	0	0	0	7	7	0	2	2	1	0	0	5	5.5
17:45	0	7	3	0	0	0	10	10	0	5	3	0	0	0	8	8
H/TOT	0	26	6	0	0	0	32	32	0	22	11	2	0	0	35	36
6 TOT	0	127	22	1	1	0	151	152.8	0	109	33	7	4	2	155	165.7



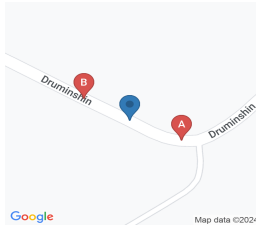
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Tue, 05 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	8	1	0	0	0	9	9	0	4	2	0	0	0	6	6
18:15	0	9	0	0	0	0	9	9	1	4	2	1	0	0	8	7.9
18:30	0	6	0	1	0	0	7	7.5	0	5	0	0	0	0	5	5
18:45	0	5	0	0	0	0	5	5	0	3	1	0	0	0	4	4
H/TOT	0	28	1	1	0	0	30	30.5	1	16	5	1	0	0	23	22.9
19:00	0	3	0	0	0	0	3	3	0	5	0	2	0	0	7	8
19:15	0	2	0	0	0	0	2	2	0	1	1	0	0	0	2	2
19:30	0	3	0	0	0	0	3	3	0	1	0	0	0	0	1	1
19:45	0	3	1	0	0	0	4	4	0	1	2	0	0	0	3	3
H/TOT	0	11	1	0	0	0	12	12	0	8	3	2	0	0	13	14
20:00	0	3	1	0	0	0	4	4	0	3	0	0	0	0	3	3
20:15	0	4	0	0	0	0	4	4	0	2	1	0	0	0	3	3
20:30	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
20:45	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
H/TOT	0	11	1	0	0	0	12	12	0	7	1	0	0	0	8	8
21:00	0	4	0	0	0	0	4	4	0	3	0	0	0	0	3	3
21:15	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
21:30	0	2	1	0	0	0	3	3	0	2	1	0	0	0	3	3
21:45	0	3	0	0	0	0	3	3	0	1	0	0	0	0	1	1
H/TOT	0	11	1	0	0	0	12	12	0	7	1	0	0	0	8	8
22:00	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
22:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:30	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
22:45	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
H/TOT	0	3	0	0	0	0	3	3	0	3	0	0	0	0	3	3
23:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
23:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:30	0	1	0	0	0	0	1	1	0	3	0	0	0	0	3	3
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	4	0	0	0	0	4	4
6 TOT	0	65	4	1	0	0	70	70.5	1	45	10	3	0	0	59	59.9
24 TOT	0	272	45	11	1	0	329	335.8	1	252	57	13	6	2	331	346.7



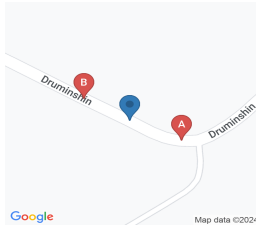
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Wed, 06 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
05:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45	0	0	0	1	0	0	1	1.5	0	2	0	0	0	0	2	2
H/TOT	0	0	0	1	0	0	1	1.5	0	3	0	0	0	0	3	3
6 TOT	0	0	0	1	0	0	1	1.5	0	3	0	0	0	0	3	3



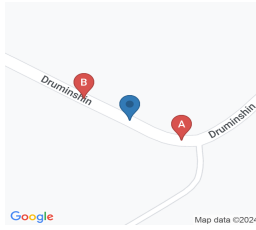
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Wed, 06 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
06:00	0	0	0	0	0	0	0	0	0	2	0	1	0	0	3	3.5
06:15	0	2	0	0	0	0	2	2	0	1	0	0	0	0	1	1
06:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45	0	3	0	0	0	0	3	3	0	4	0	0	0	0	4	4
H/TOT	0	5	0	0	0	0	5	5	0	7	0	1	0	0	8	8.5
07:00	0	4	0	0	0	0	4	4	0	1	0	0	0	0	1	1
07:15	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	2
07:30	0	4	0	0	0	0	4	4	0	2	1	0	0	0	3	3
07:45	0	6	1	1	0	0	8	8.5	0	5	1	0	0	0	6	6
H/TOT	0	14	1	1	0	0	16	16.5	0	10	2	0	0	0	12	12
08:00	0	3	1	0	1	0	5	6.3	0	8	1	0	0	0	9	9
08:15	0	9	1	0	0	0	10	10	0	7	0	0	0	0	7	7
08:30	0	8	2	0	0	0	10	10	0	12	0	3	0	0	15	16.5
08:45	0	10	2	3	0	0	15	16.5	0	9	2	0	0	0	11	11
H/TOT	0	30	6	3	1	0	40	42.8	0	36	3	3	0	0	42	43.5
09:00	0	1	1	2	0	0	4	5	0	3	2	2	0	0	7	8
09:15	0	1	2	0	0	0	3	3	0	1	1	0	0	0	2	2
09:30	0	4	2	0	0	0	6	6	0	2	0	0	0	0	2	2
09:45	0	5	1	0	0	0	6	6	0	6	0	0	0	0	6	6
H/TOT	0	11	6	2	0	0	19	20	0	12	3	2	0	0	17	18
10:00	0	2	0	0	1	0	3	4.3	0	3	1	0	0	0	4	4
10:15	0	3	1	0	0	0	4	4	0	2	2	0	1	0	5	6.3
10:30	0	5	0	1	0	0	6	6.5	0	2	1	0	0	0	3	3
10:45	0	2	1	0	0	0	3	3	0	5	0	1	0	0	6	6.5
H/TOT	0	12	2	1	1	0	16	17.8	0	12	4	1	1	0	18	19.8
11:00	0	5	1	0	0	0	6	6	0	3	2	0	0	0	5	5
11:15	0	3	0	0	0	0	3	3	0	5	2	0	0	0	7	7
11:30	0	4	0	0	0	0	4	4	0	3	1	0	0	0	4	4
11:45	0	4	0	0	0	0	4	4	0	3	1	1	0	0	5	5.5
H/TOT	0	16	1	0	0	0	17	17	0	14	6	1	0	0	21	21.5
6 TOT	0	88	16	7	2	0	113	119.1	0	91	18	8	1	0	118	123.3



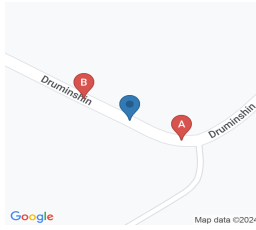
Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Wed, 06 Mar 2024

	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
12:00	0	4	3	0	0	0	7	7	0	3	3	1	0	0	7	7.5
12:15	0	2	0	0	0	0	2	2	0	2	1	0	0	0	3	3
12:30	0	7	1	1	0	0	9	9.5	0	3	1	0	0	0	4	4
12:45	0	4	1	0	0	0	5	5	0	4	0	1	0	0	5	5.5
H/TOT	0	17	5	1	0	0	23	23.5	0	12	5	2	0	0	19	20
13:00	0	5	0	0	0	0	5	5	0	5	0	0	0	0	5	5
13:15	0	6	0	0	0	0	6	6	0	2	1	0	1	0	4	5.3
13:30	0	3	1	1	0	0	5	5.5	0	4	0	1	0	0	5	5.5
13:45	0	3	0	1	0	0	4	4.5	0	3	1	1	0	0	5	5.5
H/TOT	0	17	1	2	0	0	20	21	0	14	2	2	1	0	19	21.3
14:00	0	2	1	0	0	0	3	3	0	5	2	0	0	0	7	7
14:15	0	3	0	0	0	0	3	3	0	4	3	0	0	1	8	9
14:30	0	12	1	0	0	0	13	13	0	6	1	0	0	0	7	7
14:45	0	3	1	1	1	0	6	7.8	0	2	2	0	0	0	4	4
H/TOT	0	20	3	1	1	0	25	26.8	0	17	8	0	0	1	26	27
15:00	0	3	0	0	1	0	4	5.3	0	4	1	0	0	0	5	5
15:15	0	7	3	0	0	0	10	10	0	1	1	0	0	0	2	2
15:30	0	7	0	0	1	0	8	9.3	0	7	0	2	1	1	11	14.3
15:45	0	0	0	0	0	0	0	0	0	3	1	0	0	0	4	4
H/TOT	0	17	3	0	2	0	22	24.6	0	15	3	2	1	1	22	25.3
16:00	0	6	2	0	0	0	8	8	0	6	1	0	0	0	7	7
16:15	0	1	1	0	0	0	2	2	0	6	1	0	0	0	7	7
16:30	0	5	1	0	0	0	6	6	0	8	2	0	0	0	10	10
16:45	0	8	2	0	0	0	10	10	0	3	2	0	2	0	7	9.6
H/TOT	0	20	6	0	0	0	26	26	0	23	6	0	2	0	31	33.6
17:00	0	9	1	0	0	0	10	10	0	12	3	0	0	0	15	15
17:15	0	5	2	0	0	0	7	7	0	4	0	0	1	0	5	6.3
17:30	0	3	0	0	0	0	3	3	0	2	1	0	0	0	3	3
17:45	0	4	0	0	0	0	4	4	0	5	0	0	0	0	5	5
H/TOT	0	21	3	0	0	0	24	24	0	23	4	0	1	0	28	29.3
6 TOT	0	112	21	4	3	0	140	145.9	0	104	28	6	5	2	145	156.5



Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Date: Wed, 06 Mar 2024

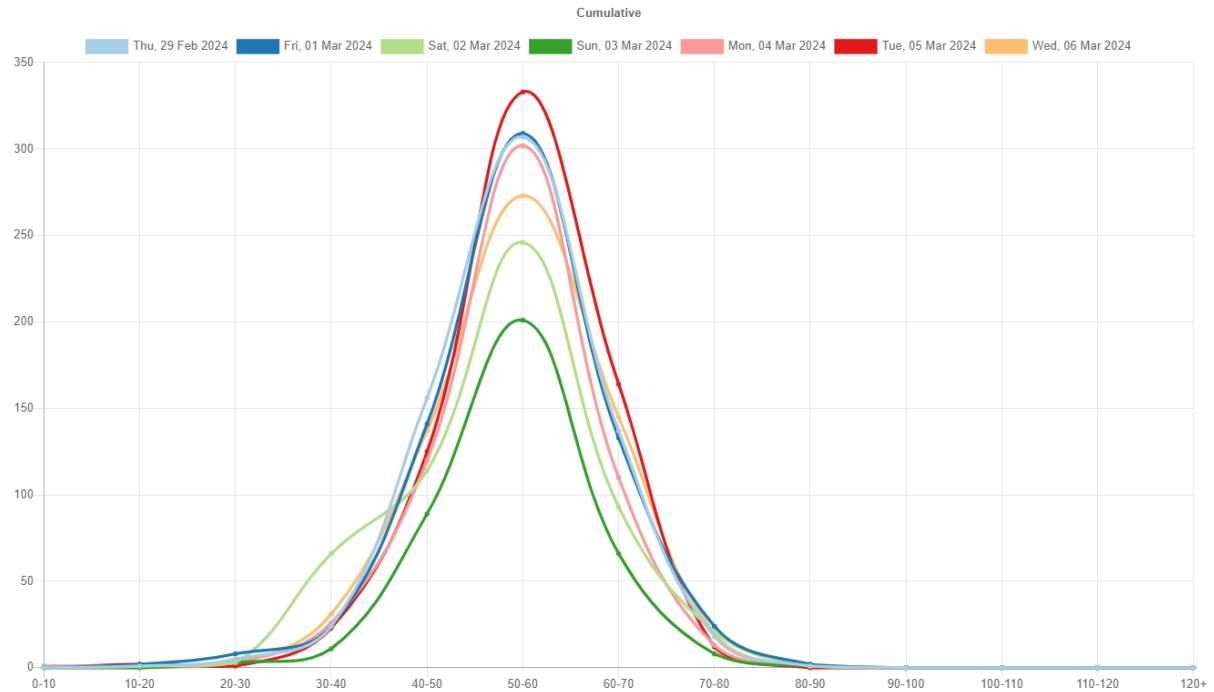
	A => B								B => A							
TIME	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU	M/C	CAR	LGV	OGV1	OGV2	PSV	TOT	PCU
18:00	0	5	1	0	0	0	6	6	0	6	1	1	0	0	8	8.5
18:15	0	1	0	0	0	0	1	1	0	5	1	0	0	0	6	6
18:30	0	4	2	0	0	0	6	6	0	1	1	0	0	0	2	2
18:45	0	2	0	0	0	0	2	2	0	8	1	0	0	0	9	9
H/TOT	0	12	3	0	0	0	15	15	0	20	4	1	0	0	25	25.5
19:00	0	2	0	0	0	0	2	2	0	4	0	0	0	0	4	4
19:15	0	2	0	0	0	0	2	2	0	3	0	1	1	0	5	6.8
19:30	0	4	0	0	0	0	4	4	0	2	0	0	0	0	2	2
19:45	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
H/TOT	0	10	0	0	0	0	10	10	0	9	0	1	1	0	11	12.8
20:00	0	3	0	0	0	0	3	3	0	1	0	0	0	0	1	1
20:15	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
20:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
20:45	0	3	0	0	0	0	3	3	0	1	0	0	0	0	1	1
H/TOT	0	8	0	0	0	0	8	8	0	4	0	0	0	0	4	4
21:00	0	2	1	0	0	0	3	3	0	2	0	0	0	0	2	2
21:15	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0
21:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
21:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	4	1	0	0	0	5	5	0	3	0	0	0	0	3	3
22:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	1
22:30	0	1	1	0	0	0	2	2	0	0	0	0	0	0	0	0
22:45	0	2	0	0	0	0	2	2	0	0	0	0	0	0	0	0
H/TOT	0	3	1	0	0	0	4	4	0	1	0	0	0	0	1	1
23:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:30	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
23:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
H/TOT	0	1	0	0	0	0	1	1	0	1	0	0	0	0	1	1
6 TOT	0	38	5	0	0	0	43	43	0	38	4	2	1	0	45	47.3
24 TOT	0	238	42	12	5	0	297	309.5	0	236	50	16	7	2	311	330.1

Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Speed Bins Cumulative



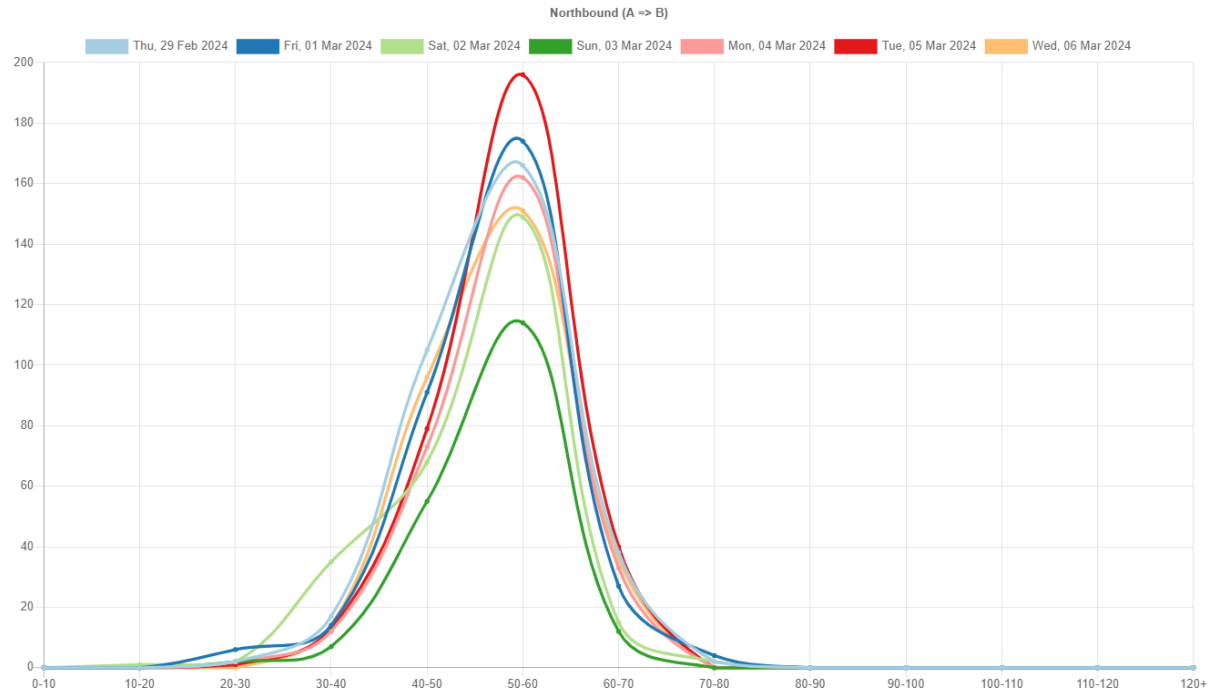
	No. in Range						
Speed KPH	Thu 29 Feb 2024	Fri 01 Mar 2024	Sat 02 Mar 2024	Sun 03 Mar 2024	Mon 04 Mar 2024	Tue 05 Mar 2024	Wed 06 Mar 2024
0-10	0	0	0	0	1	0	0
10-20	1	2	1	0	1	2	0
20-30	5	8	3	3	3	1	2
30-40	24	24	66	11	26	23	31
40-50	156	141	114	89	120	125	137
50-60	307	309	246	201	302	333	273
60-70	137	133	93	66	110	164	145
70-80	18	24	21	8	13	12	20
80-90	1	2	1	1	1	0	0
90-100	0	0	0	0	0	0	0
100-110	0	0	0	0	0	0	0
110-120	0	0	0	0	0	0	0
120+	0	0	0	0	0	0	0

Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Druminshin

Speed Bins Northbound (A => B)



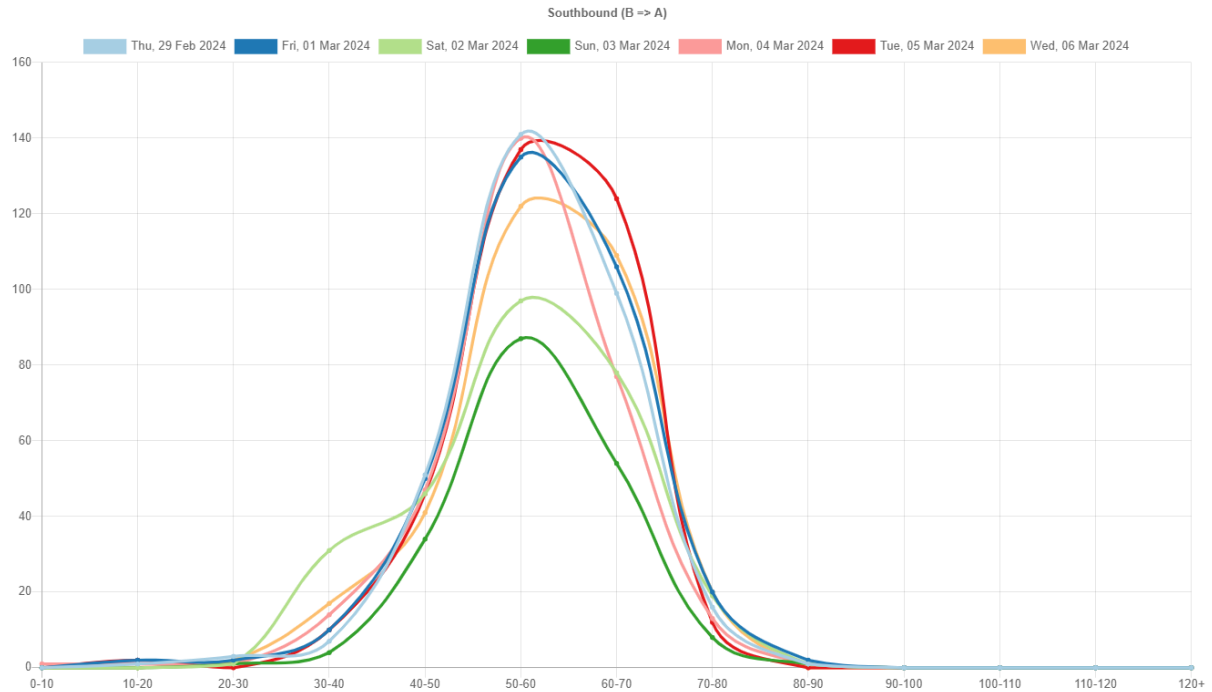
	No. in Range						
Speed KPH	Thu 29 Feb 2024	Fri 01 Mar 2024	Sat 02 Mar 2024	Sun 03 Mar 2024	Mon 04 Mar 2024	Tue 05 Mar 2024	Wed 06 Mar 2024
0-10	0	0	0	0	0	0	0
10-20	0	0	1	0	0	0	0
20-30	2	6	2	2	2	1	0
30-40	17	14	35	7	12	13	14
40-50	105	91	68	55	73	79	96
50-60	166	174	149	114	162	196	151
60-70	38	27	15	12	33	40	36
70-80	2	4	2	0	0	0	0
80-90	0	0	0	0	0	0	0
90-100	0	0	0	0	0	0	0
100-110	0	0	0	0	0	0	0
110-120	0	0	0	0	0	0	0
120+	0	0	0	0	0	0	0

Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Drumminshin

Speed Bins Southbound (B => A)






































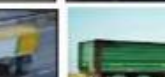







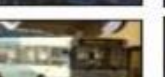
	No. in Range						
Speed KPH	Thu 29 Feb 2024	Fri 01 Mar 2024	Sat 02 Mar 2024	Sun 03 Mar 2024	Mon 04 Mar 2024	Tue 05 Mar 2024	Wed 06 Mar 2024
0-10	0	0	0	0	1	0	0
10-20	1	2	0	0	1	2	0
20-30	3	2	1	1	1	0	2
30-40	7	10	31	4	14	10	17
40-50	51	50	46	34	47	46	41
50-60	141	135	97	87	140	137	122
60-70	99	106	78	54	77	124	109
70-80	16	20	19	8	13	12	20
80-90	1	2	1	1	1	0	0
90-100	0	0	0	0	0	0	0
100-110	0	0	0	0	0	0	0
110-120	0	0	0	0	0	0	0
120+	0	0	0	0	0	0	0

Survey Name: 24130 - Loughbrackan, Co. Meath ATC

Site: ATC 1

Location: Druminshin

COBA Classification

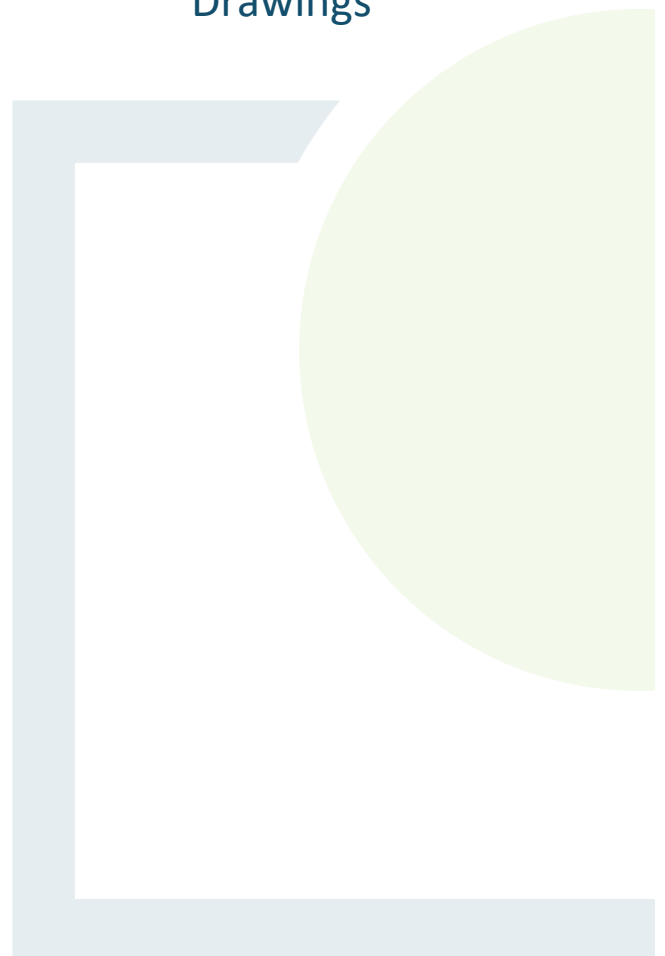
CAR	         
LGV	       
OGV 1	         
OGV 2	       
Tractor with Trailers – OGV2	
BUS	       

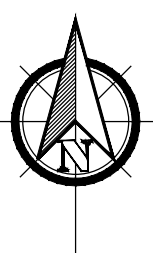
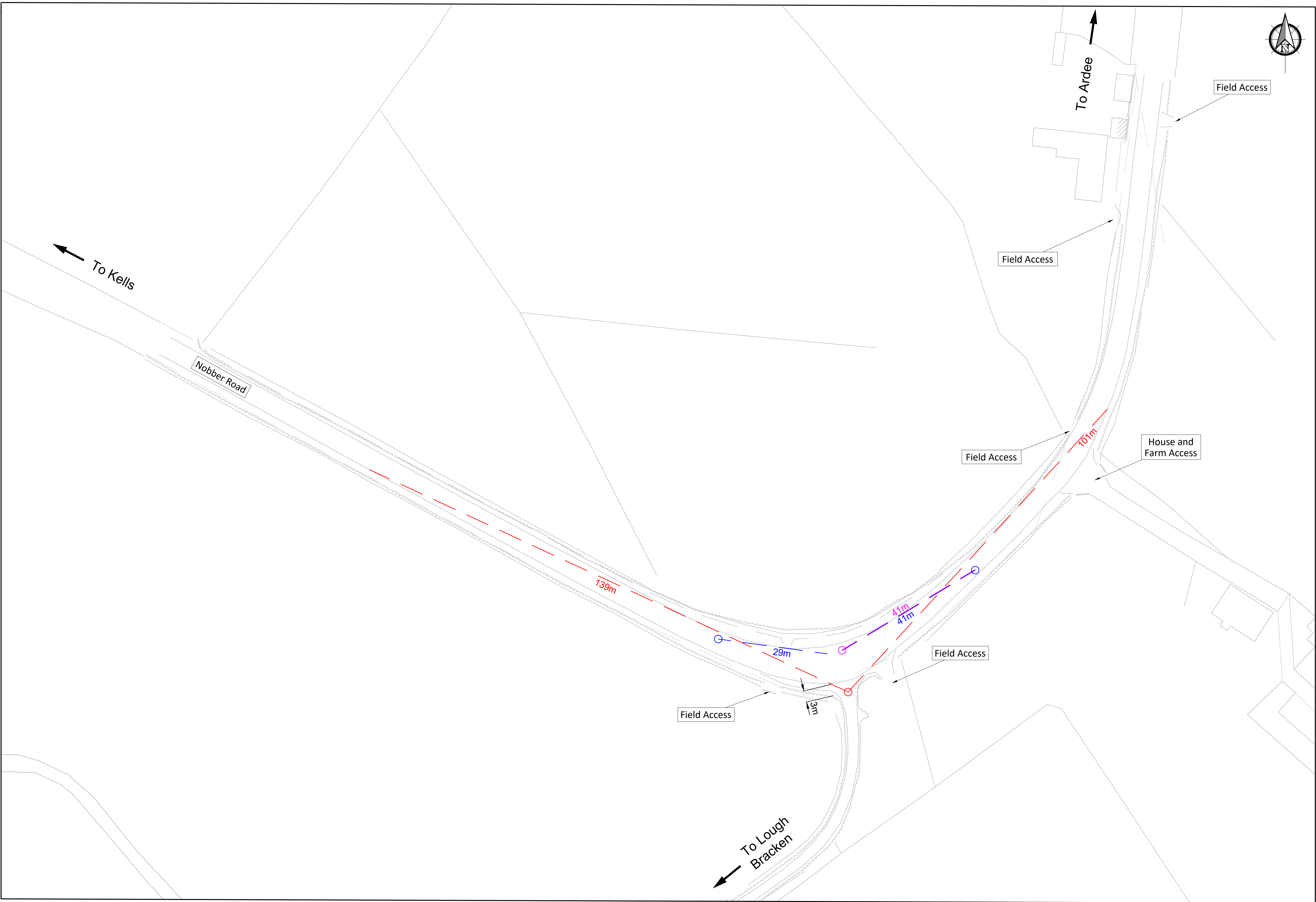


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APPENDIX C

Junction Visibility and
Stopping Sight Distance
Drawings

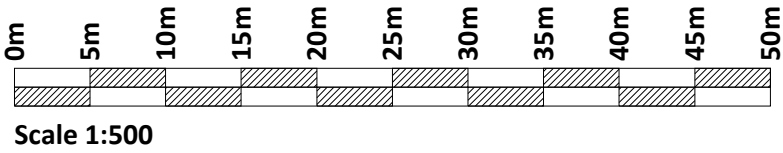




- Legend**
- Junction Visibility - Vehicle Exiting Development Site
 - Through Vehicle Stopping Sight Distance - Eastbound/Westbound Vehicles on Nobber Road
 - Turning Vehicle Stopping Sight Distance - Vehicle Turning into Development Site
 - Position of Vehicle - Driver Visibility from Vehicle to end of Corresponding Sightline




Note:
Speed Limit of 80km/h corresponds to a Design Speed of 85km/h

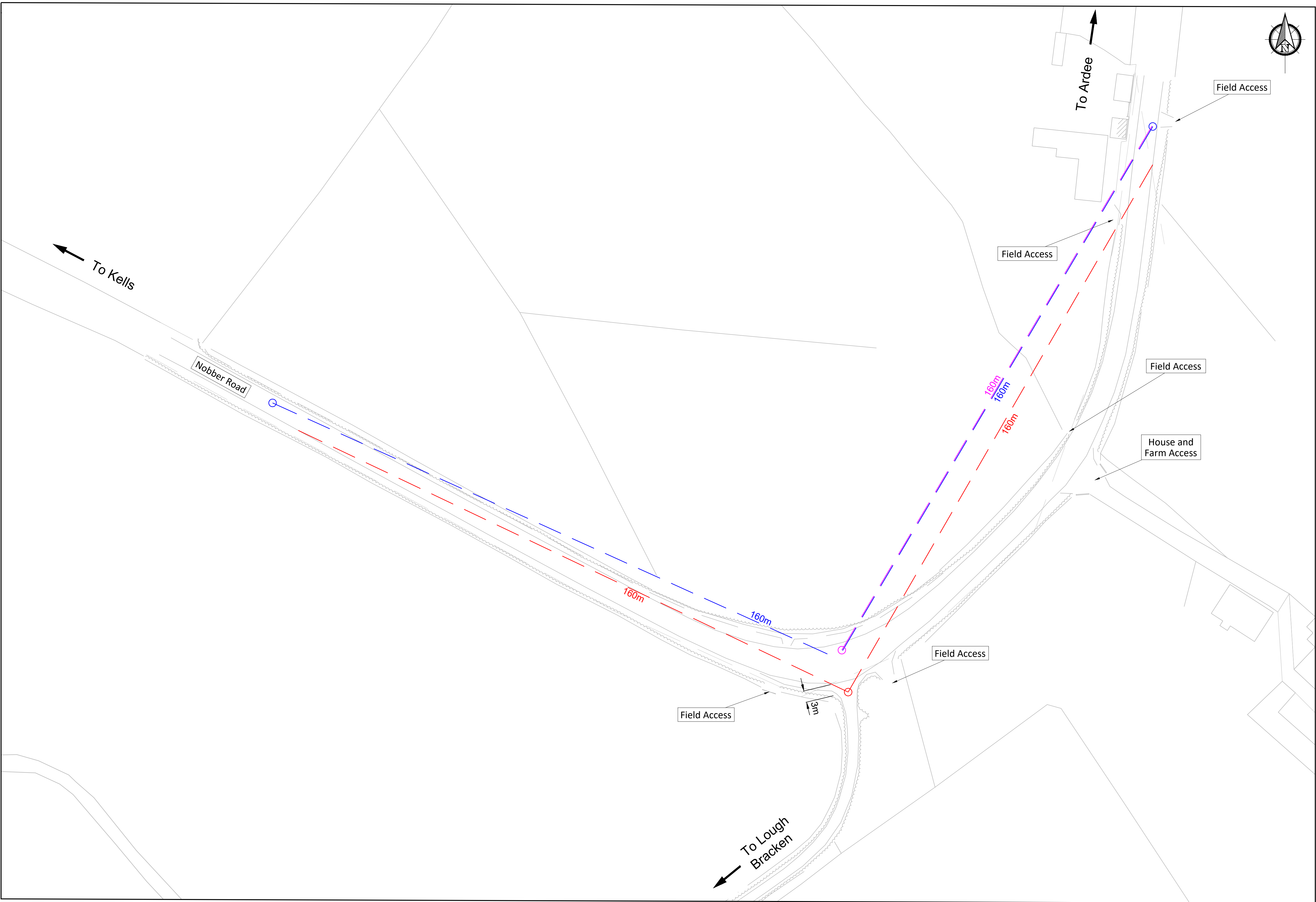
Scale 1:500



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Rev.	Description	App By	Date
A	ISSUE FOR INFORMATION	GF	17.05.24

PROJECT	DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH			CLIENT <div><div>comhairle chontae na mí meath county council</div></div>					
SHEET	JUNCTION VISIBILITY ANALYSIS: EXTENTS OF EXISTING SIGHT DISTANCE			Date	17.05.24	Project number	P23-013	Scale (@ A1-)	1:500
				Drawn by	AQ	Drawing Number			Rev
				Checked by	PW	P23-013-0101-0001			A

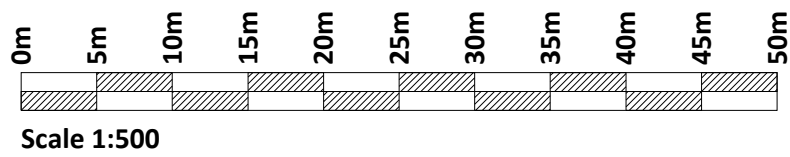


- Legend**
- Junction Visibility - Vehicle Exiting Development Site
 - Through Vehicle Stopping Sight Distance - Eastbound/Westbound Vehicles on Nobber Road
 - Turning Vehicle Stopping Sight Distance - Vehicle Turning into Development Site
 - ○ ○ Position of Vehicle - Driver Visibility from Vehicle to end of Corresponding Sightline

Note:
Speed Limit of 80km/h corresponds to a Design Speed of 85km/h

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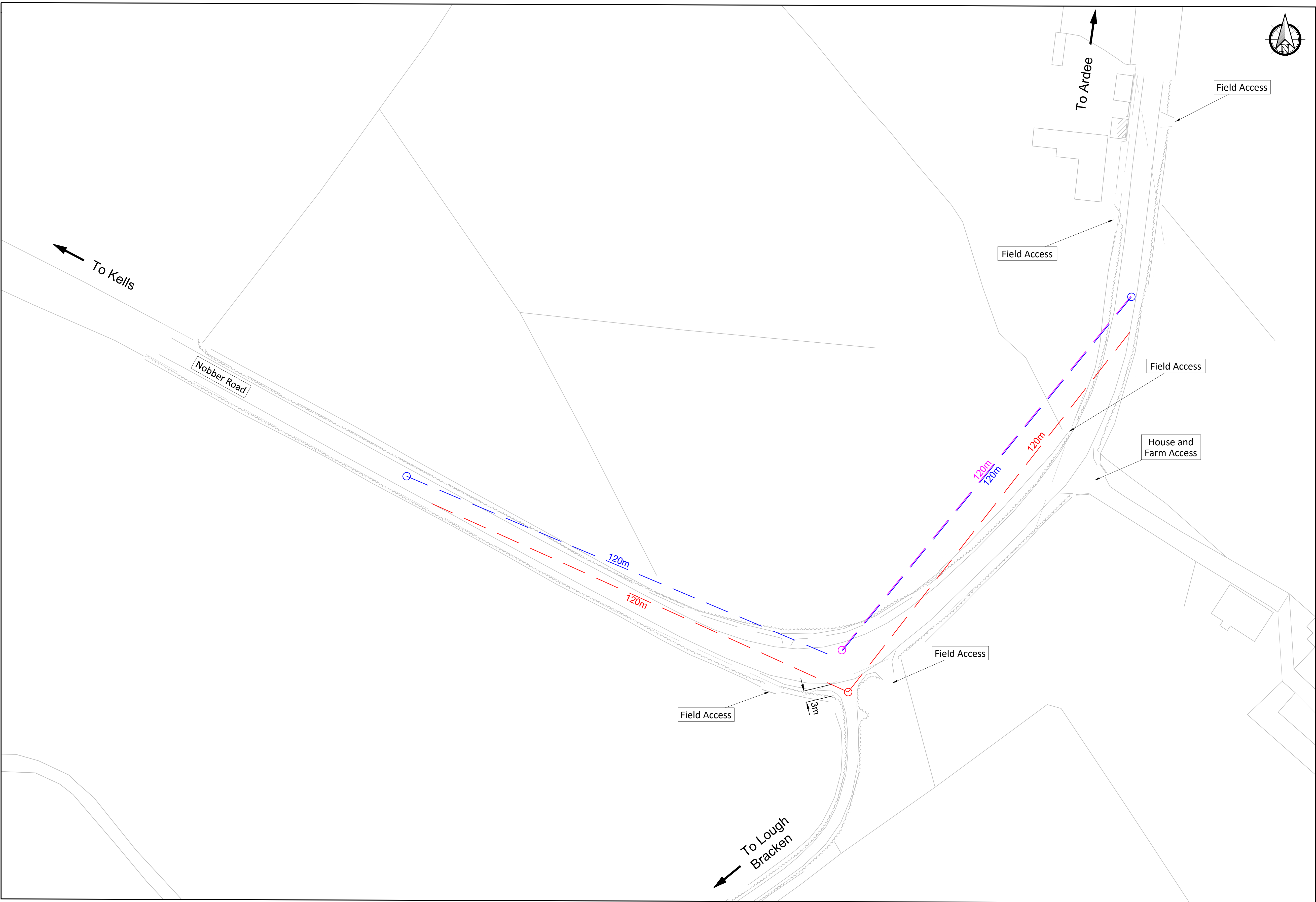
www.fehilytimoney.ie

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Rev.	Description	App By	Date
A	ISSUE FOR INFORMATION	GF	17.05.24

PROJECT		CLIENT		
DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		 comhairle chontae na mí meath county council		
SHEET		Date	Project number	Scale (@ A1-)
JUNCTION VISIBILITY ANALYSIS: SIGHT DISTANCE REQUIREMENTS FOR AN 85KM/H DESIGN SPEED		17.05.24	P23-013	1:500
		Drawn by	Drawing Number	
		AQ	P23-013-0101-0002	
		Checked by	PW	Rev
				A

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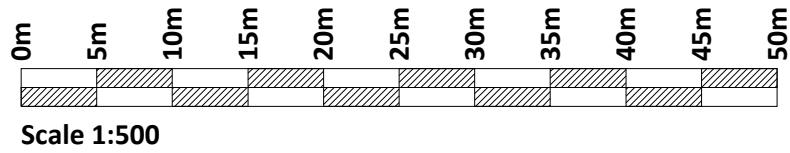


- Legend
- Junction Visibility - Vehicle Exiting Development Site
 - Through Vehicle Stopping Sight Distance - Eastbound/Westbound Vehicles on Nobber Road
 - Turning Vehicle Stopping Sight Distance - Vehicle Turning into Development Site
 - Position of Vehicle - Driver Visibility from Vehicle to end of Corresponding Sightline

Note:
Speed Limit of 60km/h corresponds to a Design Speed of 70km/h

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A	ISSUE FOR INFORMATION	GF	17.05.24

PROJECT		CLIENT		
DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		 comhairle chontae na mí meath county council		
SHEET		Date	Project number	Scale (@ A1-)
JUNCTION VISIBILITY ANALYSIS SIGHT DISTANCE REQUIREMENTS FOR A 70KM/H DESIGN SPEED		17.05.24	P23-013	1:500
		Drawn by	Drawing Number	
		AQ	P23-013-0101-0003	
		Checked by	PW	Rev
				A

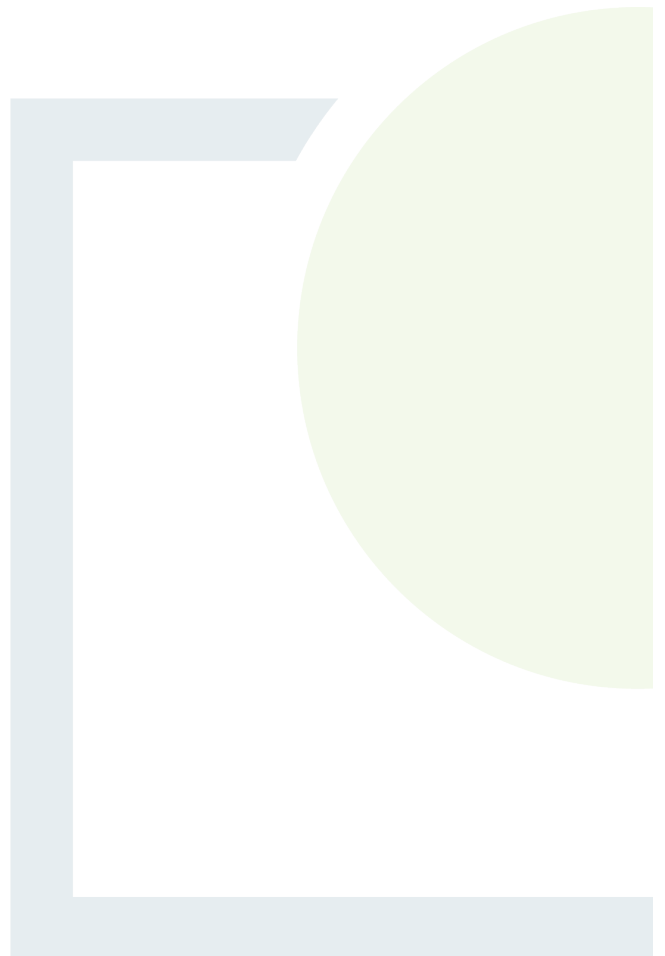
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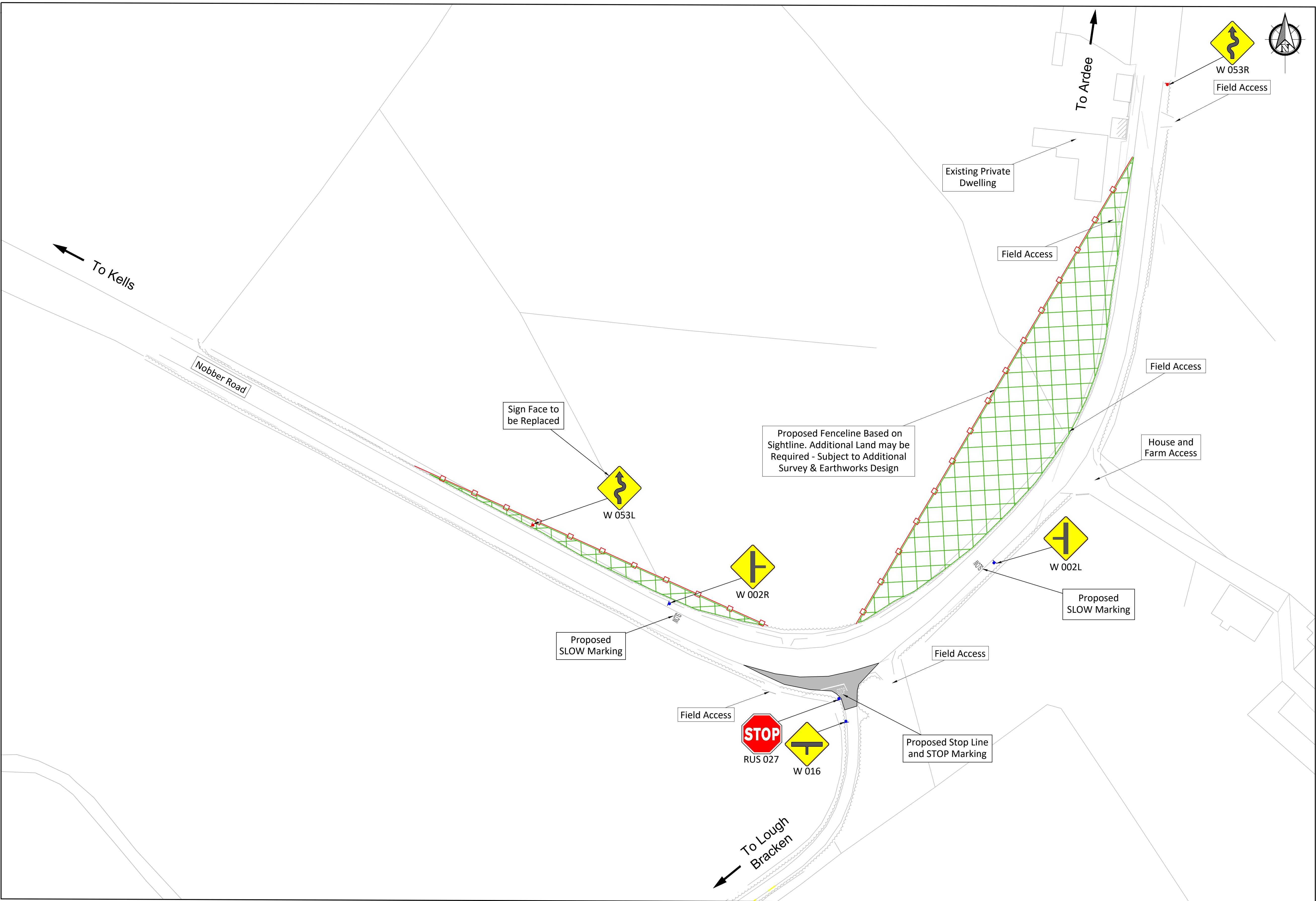


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APPENDIX **D**

Proposed Upgrade Drawings





Legend

Area to be Cleared

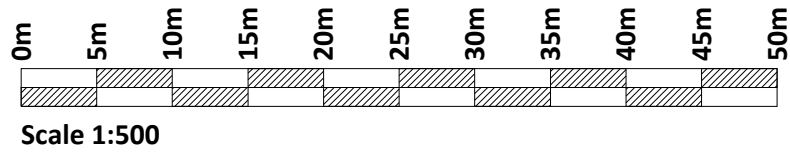
Area to be Paved

Proposed Fence

Proposed Signage

Existing Signage

Note:
Speed Limit of 80km/h corresponds to a Design Speed of 85km/h



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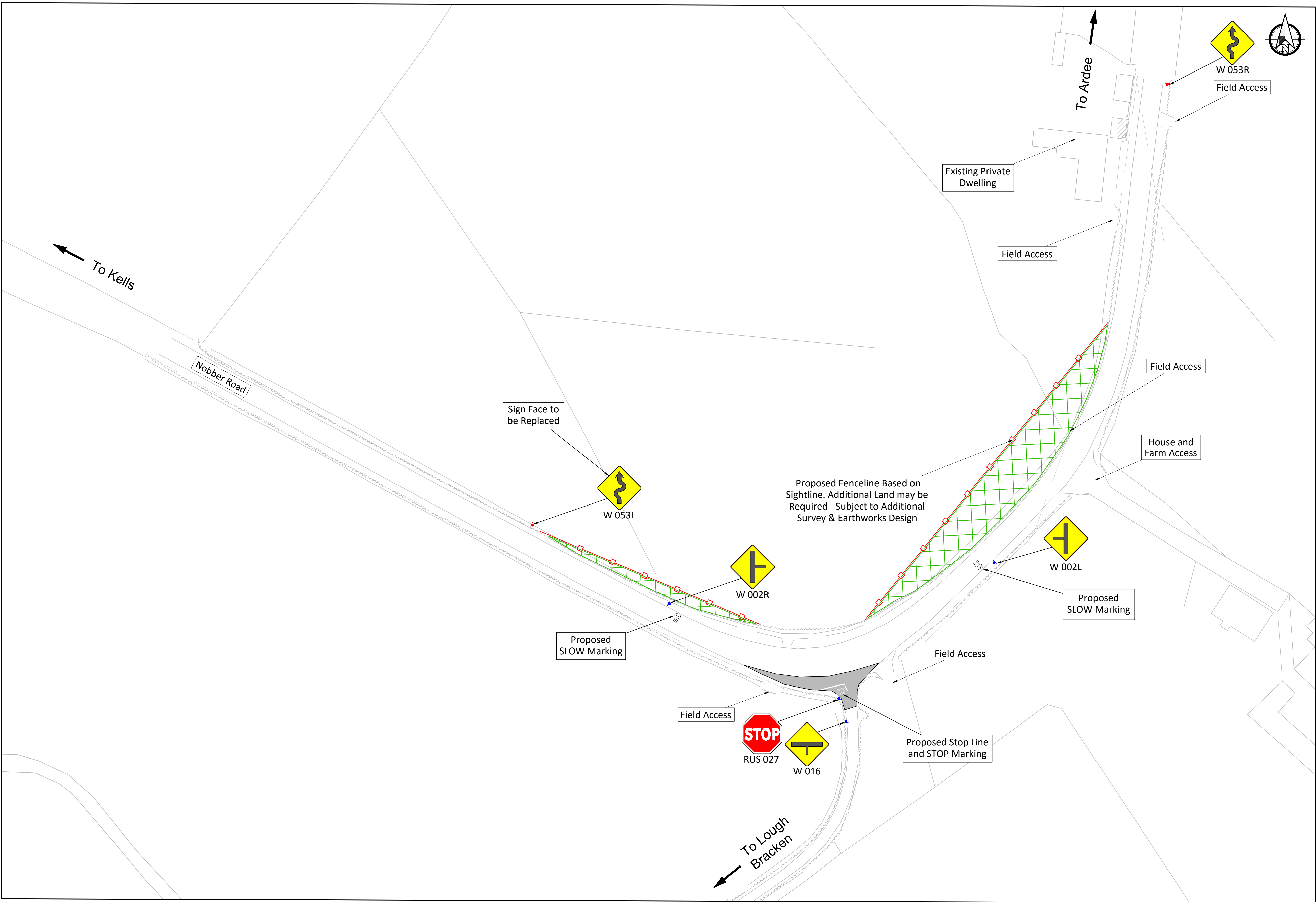
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A	ISSUE FOR INFORMATION	GF	17.05.24

PROJECT		DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		CLIENT		<div><div><div></div><div></div><div></div></div><div>comhairle chontae na mí meath county council</div></div>				
SHEET	PROPOSED UPGRADE MEASURES 85KM/H DESIGN SPEED			Date	17.05.24	Project number	P23-013	Scale (@ A1-)	1:500	
				Drawn by	AQ	Drawing Number P23-013-0101-0004				Rev
				Checked by	PW					A

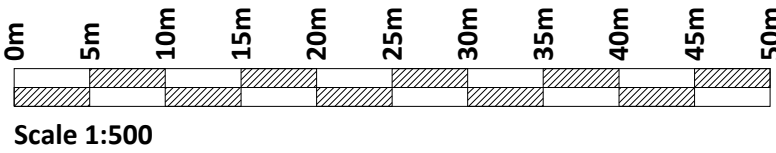
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Legend

- Area to be Cleared
- Area to be Paved
- Proposed Fence
- Proposed Signage
- Existing Signage

Note:
Speed Limit of 60km/h corresponds to a Design Speed of 70km/h



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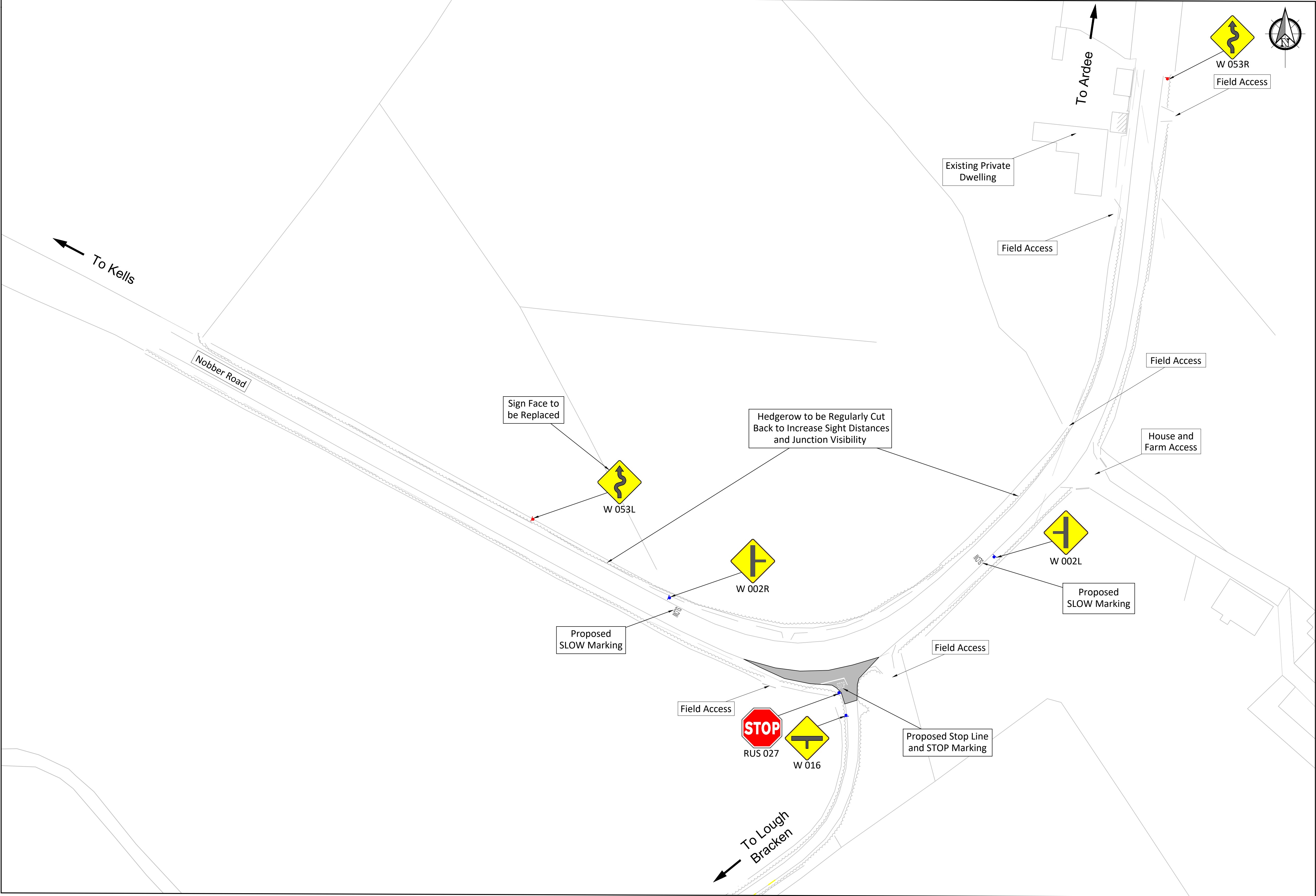
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A	ISSUE FOR INFORMATION	GF	17.05.24

PROJECT	DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		CLIENT						
			<div><div></div><div>comhairle chontae na mí meath county council</div></div>						
SHEET	PROPOSED UPGRADE MEASURES 70KM/H DESIGN SPEED		Date	17.05.24	Project number	P23-013	Scale (@ A1-)	1:500	
			Drawn by	AQ	Drawing Number P23-013-0101-0005			Rev	A
			Checked by	PW					

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Legend

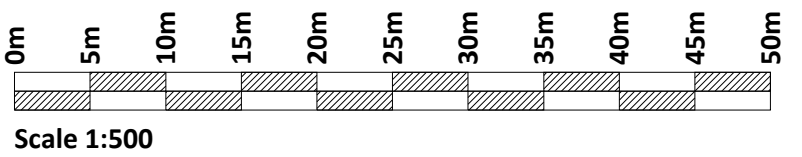
Area to be Paved

Proposed Signage

Existing Signage

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PROJECT	DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		CLIENT					
			<div><div></div><div>comhairle chontae na mí meath county council</div></div>					
SHEET	PROPOSED UPGRADE MEASURES NO LAND ACQUISITION		Date	17.05.24	Project number	P23-013	Scale (@ A1-)	1:500
			Drawn by	AQ	Drawing Number			Rev
			Checked by	PW	P23-013-0101-0006			A

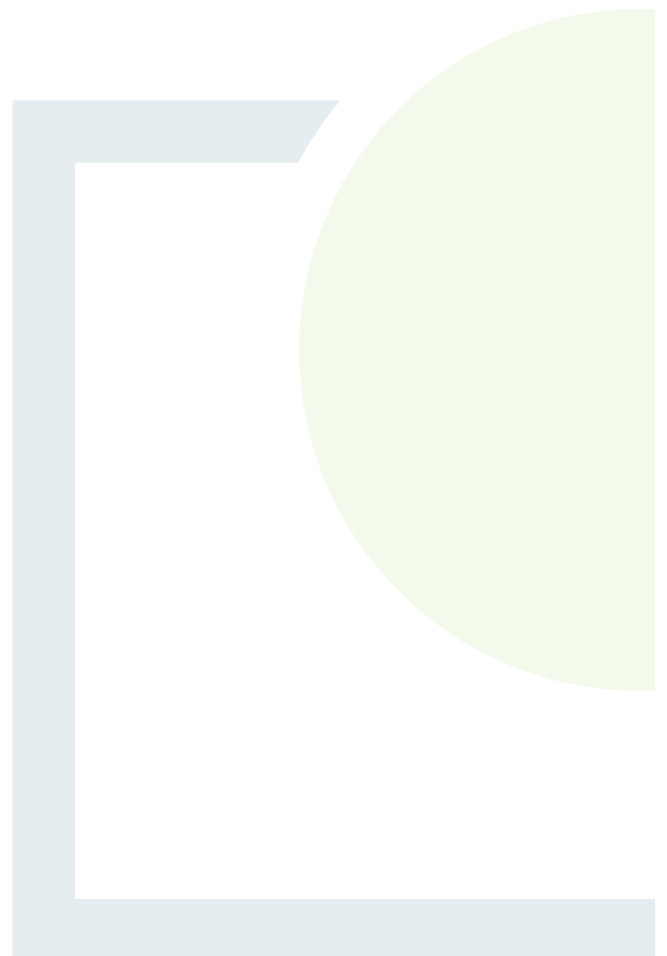
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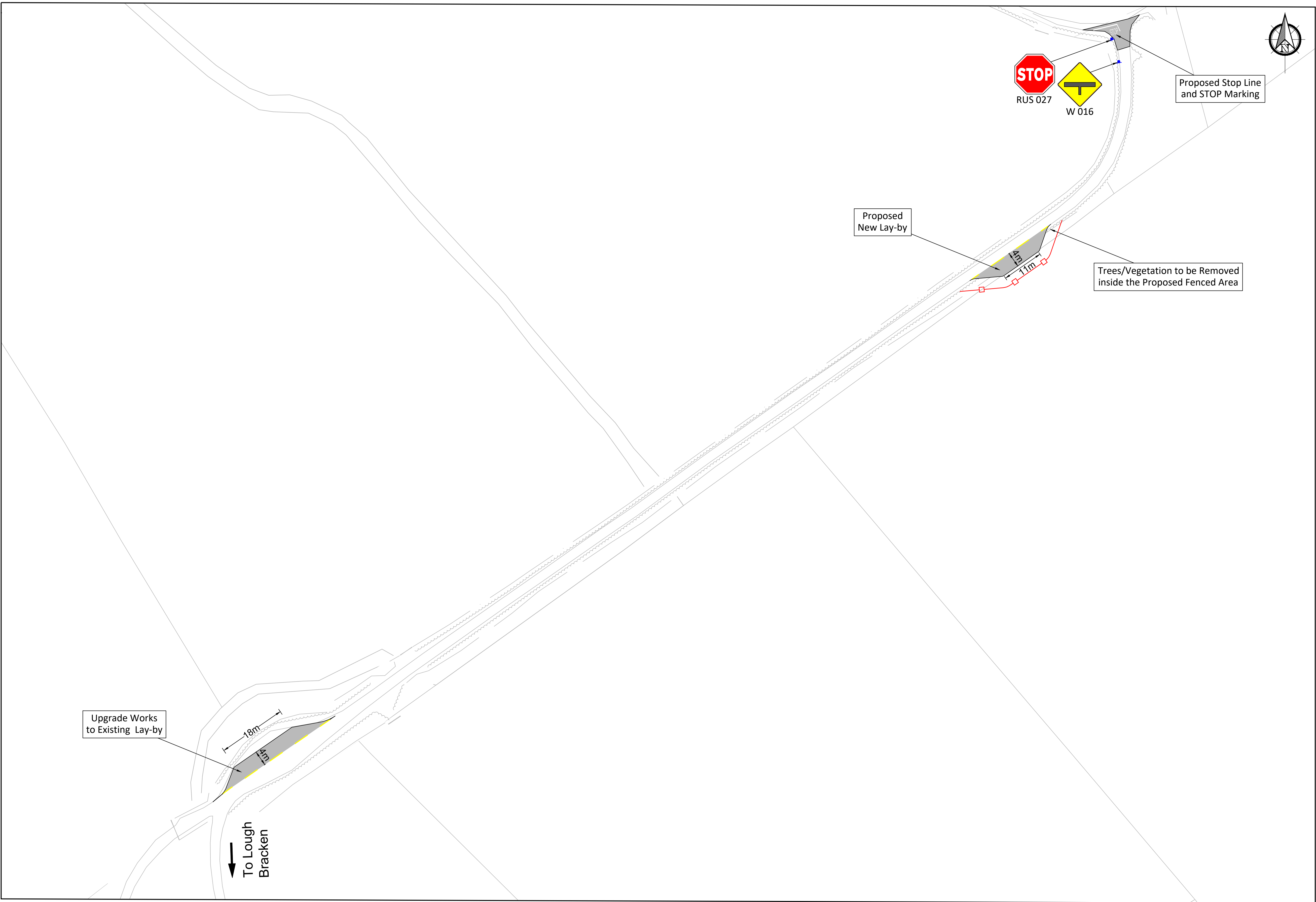


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APPENDIX E

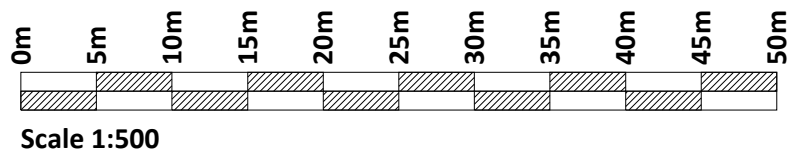
Proposed Lay-by





Legend

- Area to be Paved
- Proposed Fence
- Proposed Signage
- Existing Signage



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Scale 1:500






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PROJECT	DEVELOPMENT AT LOUGH BRACKEN, CO. MEATH		CLIENT					
			<div><div></div><div><div>comhairle chontae na mí</div><div>meath county council</div></div></div>					
SHEET	PROPOSED LAYOUT OF LAY-BYS		Date	17.05.24	Project number	P23-013	Scale (@ A1-)	1:500
			Drawn by	AQ	Drawing Number			Rev
			Checked by	PW	P23-013-0101-0007			A

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